

GENERAL CONSTRUCTION NOTES

- ALL CONSTRUCTION WORK FOR THIS PROJECT SHALL CONFORM TO THE LATEST EDITION OF THE LOUDOUN WATER STANDARDS AND SPECIFICATIONS AS WELL AS VIRGINIA DEPARTMENT OF TRANSPORTATION "ROAD AND BRIDGE SPECIFICATIONS," 2007 (OR LATEST) WHERE SPECIFIED.
- THE CONTRACTOR SHALL PROVIDE PEDESTRIAN ACCESS THROUGH THE SITE AS DIRECTED BY THE ENGINEER AT ALL TIMES DURING CONSTRUCTION AND SHALL ENSURE THE SAFETY OF PEDESTRIANS FROM TRAFFIC.
- THE COST OF MOVING AND RESETTING OF LANDSCAPING; REMOVAL AND REPLACEMENT OF ALL SITE FEATURES SUCH AS MAIL BOXES, DRIVEWAYS, CURBS, GUTTERS AND SIDEWALKS; AND RESTORATION OF PAVEMENT, WHERE REQUIRED, SHALL BE INCLUDED IN THE UNIT BID PRICE FOR PIPE IN PLACE.
- ALL UNSUITABLE BACKFILL MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE. SUITABLE BACKFILL MATERIAL MAY BE REUSED ANYWHERE ON THE JOB AND MAY BE STOCKPILED FOR REUSE. STORING, TRANSPORTATION, LOADING AND OTHER ASSOCIATED COSTS ARE TO BE INCLUDED IN THE BID PRICE FOR PIPE IN PLACE.
- AT THE OPEN CUT SECTIONS, CONCRETE CRADLES SHALL BE PROVIDED AT ALL STORMWATER/SANITARY CROSSINGS. IF SO DIRECTED BY THE ENGINEER, ADDITIONAL CONCRETE CRADLES, PIERS AND ENCASEMENTS SHALL BE PROVIDED TO PROTECT OTHER UTILITIES WITHIN THE CONSTRUCTION LIMITS. THE CONCRETE CRADLES, PIERS AND ENCASEMENTS SHALL BE PAID FOR AT THE STIPULATED UNIT PRICE.
- THE COST OF REMOVAL OF ANY SIDEWALKS, CONCRETE, ASPHALT, ETC. IS TO BE INCLUDED IN THE UNIT BID PRICE FOR PIPE IN PLACE.
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING THE REQUIRED VDOT, LAND DISTURBANCE AND SEDIMENTATION AND EROSION CONTROL PERMITS ON BEHALF OF THE TOWN OF MIDDLEBURG FOR THIS PROJECT.
- CONTRACTOR SHALL PROVIDE TEMPORARY SHEETING, SHORING AND BRACING DURING CONSTRUCTION OF OPEN CUT SECTIONS AS PER LOUDOUN COUNTY SANITATION AUTHORITY STANDARDS AND SPECIFICATIONS.

UTILITIES

- THE UTILITY INFORMATION SHOWN ON THESE PLANS IS TAKEN FROM INFORMATION RECEIVED FROM THE UTILITY COMPANIES. THE TOWN OF MIDDLEBURG DOES NOT GUARANTEE THAT THE UTILITY INFORMATION SHOWN ON PLANS IS COMPLETE OR ACCURATE. THE CONTRACTOR MUST VERIFY THE UTILITY LOCATIONS PRIOR TO CONSTRUCTION.
- ALL EXISTING UNDERGROUND UTILITIES SHALL BE MARKED IN THE FIELD BY MISS UTILITY PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SCHEDULING THE FIELD MARKING OF UTILITIES WITH MISS UTILITY.
- ALL EXISTING UNDERGROUND UTILITIES SHALL BE PHYSICALLY LOCATED BY THE CONTRACTOR PRIOR TO THE BEGINNING OF ANY CONSTRUCTION IN THE VICINITY OF THESE UTILITIES.
- FOR MARKING LOCATIONS OF EXISTING UNDERGROUND UTILITY FACILITIES (GAS, TELEPHONE, ELECTRIC AND CABLE TV), CONTRACTOR SHALL NOTIFY "MISS UTILITY" AT 811 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION.

ROAD CONSTRUCTION NOTES

- ALL CONSTRUCTION, INCLUDING ANY PROPOSED LANDSCAPING, SHALL CONFORM TO THE CURRENT EDITION OF THE LOUDOUN WATER ENGINEERING DESIGN MANUAL AND THE VIRGINIA DEPARTMENT OF TRANSPORTATION (V.D.O.T.) STANDARDS AND SPECIFICATIONS.
- ALL CONSTRUCTION WITHIN THE V.D.O.T. RIGHT-OF-WAY SHALL CONFORM TO THE PROVISIONS CONTAINED IN THE V.D.O.T. LAND USE PERMIT ISSUED FOR THIS LOCATION. THE CONTRACTOR SHALL BE THOROUGHLY FAMILIARIZED WITH THE REQUIREMENTS OF THIS LAND USE PERMIT PRIOR TO THE START OF ANY CONSTRUCTION IN V.D.O.T. RIGHT-OF-WAY.
- UNLESS MORE STRINGENT COMPACTION REQUIREMENTS ARE INDICATED ON THE PLANS OR IN THE SPECIFICATIONS, THE BACKFILL OF ALL EMBANKMENT MATERIAL, THE INSTALLATION OF TRENCH BACKFILL AND THE RESTORATION OF DISTURBED AREAS SHALL BE COMPACTED TO A MINIMUM OF 95% THEORETICAL MAXIMUM DRY DENSITY AT OPTIMUM MOISTURE CONTENT WITHIN THE TOLERANCE SPECIFIED IN THE CURRENT EDITION OF THE V.D.O.T. ROAD AND BRIDGE SPECIFICATIONS.
- ALL SUBGRADE, SUBBASE, BASE AND SHOULDER MATERIAL SHALL BE PLACED AND COMPACTED TO A MINIMUM OF 95% MAXIMUM DRY DENSITY AT OPTIMUM MOISTURE CONTENT WITHIN THE TOLERANCE SPECIFIED IN THE CURRENT EDITION OF THE V.D.O.T. ROAD AND BRIDGE SPECIFICATIONS.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS OF CLEANING TRUCKS AND OR OTHER EQUIPMENT OF MUD PRIOR TO ENTERING THE V.D.O.T. RIGHT-OF-WAY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CLEAN ALL STREETS AND ALLAY DUST AND TO TAKE WHATEVER MEASURES NECESSARY TO INSURE THE ROAD(S) ARE MAINTAINED IN A CLEAN, MUD AND DUST FREE CONDITION AT ALL TIMES.
- TEMPORARY OR PERMANENT PAVEMENT PATCHES ARE TO BE PLACED IN ALL ROADWAY CUTS WITH HOT MIX THE SAME DAY THE CUT IS MADE IN ACCORDANCE WITH V.D.O.T. REQUIREMENTS.
- THE TOP ELEVATION OF EXISTING MANHOLES SHALL BE ADJUSTED TO MEET THE FINAL PAVEMENT ELEVATION.
- ADJUST MANHOLE, VALVE, AND METER BOX COVERS BEFORE PLACING ASPHALT SEAL CAP.
- THE CONTRACTOR SHALL VISIT THE SITE AND SHALL VERIFY EXISTING CONDITIONS PRIOR TO SUBMITTING A BID FOR THE CONSTRUCTION OF THE PROJECT. THE BID SHALL INCORPORATE ALL REPAIR, REPLACEMENT AND GENERAL RESTORATION WORK REQUIRED. ADDITIONAL COSTS WILL NOT BE PERMISSIBLE DUE TO FAILURE TO INCORPORATE THIS ASSOCIATED WORK INTO THE BID.
- WHERE EXISTING NATURAL DRAINAGE DITCHES OR STREAM BANKS ARE DISTURBED DURING CONSTRUCTION THE CONTRACTOR SHALL RESTORE THIS AREA TO ORIGINAL ALIGNMENT, GRADE AND INVERT.
- THE FOLLOWING PROVISIONS SHALL APPLY TO THE USE OF SHEETING AND SHORING:
 - SHEETING AND SHORING OR OTHER APPROVED METHODS FOR TRENCH BRACING WILL BE REQUIRED ON THIS CONTRACT AS NEEDED TO MEET ALL SAFETY REQUIREMENTS.
 - UNLESS OTHERWISE DIRECTED BY THE ENGINEER, SHEETING AND SHORING WILL BE REMOVED FROM ALL TRENCHES PRIOR TO BACKFILLING OPERATIONS.
 - UNLESS SPECIFICALLY IDENTIFIED IN THE CONTRACT DOCUMENTS, NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR SHEETING AND SHORING.
- THE CONTRACTOR SHALL RESTORE ALL DRIVEWAYS DISTURBED DURING CONSTRUCTION. RESTORATION SHALL CONSIST OF THE FOLLOWING:
 - * GRAVEL DRIVEWAY - PROVIDE MINIMUM 6" COMPACTED 21A
 - * ASPHALT DRIVEWAY - PROVIDE 6" COMPACTED 21A WITH MINIMUM SM-9.0A OR SM-9.5A OVERLAY
 - * CONCRETE DRIVEWAY - PROVIDE A NEAT SAWCUT CONNECTION, MINIMUM 4" COMPACTED 21A AND 5" CLASS A 3 CONCRETE WITH WWF 6X6 - W2.9X2.9
- WHERE A PROPOSED PIPE CROSSES OR PARALLELS A STREET, THE ASPHALT SHALL BE NEATLY SAW CUT. AFTER INSTALLATION OF THE PIPE, THE ROADWAY SHALL BE PATCHED IN ACCORDANCE WITH THE COUNTY AND V.D.O.T. STANDARDS AND SPECIFICATIONS.

TOWN OF MIDDLEBURG



SHEET INDEX

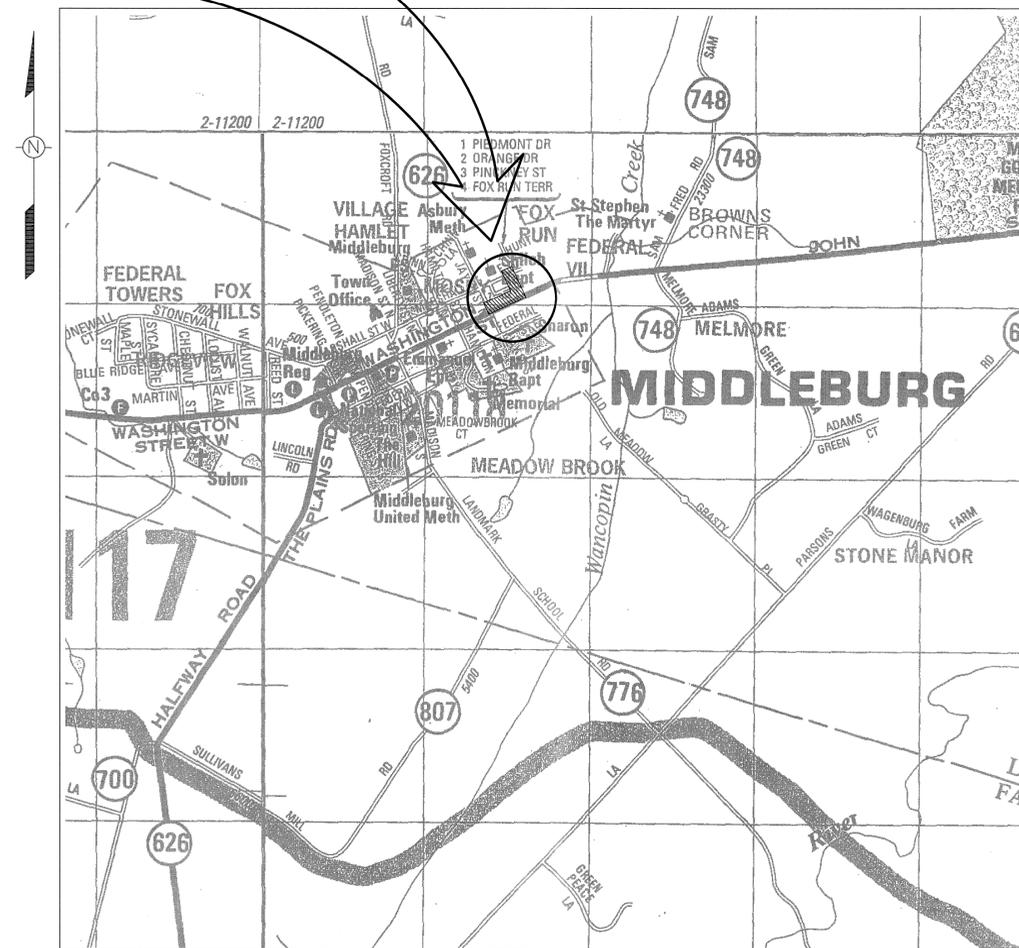
- G-1 COVER SHEET
- C-1 WATER MAIN PLAN - EAST WASHINGTON STREET AND NORTH PINCKNEY STREET
- C-2 WATER MAIN PROFILE - EAST WASHINGTON STREET
- C-3 WATER MAIN PROFILES - NORTH PINCKNEY STREET
- C-4 EROSION AND SEDIMENT CONTROL NOTES AND DETAILS
- C-5 & C-6 CIVIL AND WATER DETAILS
- T-1 TO T-6 TRAFFIC MANAGEMENT PLAN

LEGEND

- W—W— EX. WATER
- 10" SAN— EX. SEWER
- G— EX. GAS
- OHE— EX. OVERHEAD ELECTRIC
- OHW— EX. OVERHEAD WIRE
- DUCT (F.O.) EX. UNDERGROUND FO DUCT
- █ EX. STORM DRAIN
- ← PROP. WATER MAIN
- - - EX. EDGE OF PAVEMENT
- ⊗ EX. WATER METER
- ⊗ PROP. WATER METER
- ⊙ EX. VALVE
- ⊙ PROP. VALVE
- ⊙ EX. FIRE HYDRANT
- ⊗ PROP. FIRE HYDRANT
- ⊙ EX. MANHOLE
- ▨ INLET PROTECTION
- ⌋ PROP. CAP
- ∅ EX. UTILITY POLE
- - - PROPERTY LINE
- - - PERMANENT EASEMENT
- - - TEMPORARY EASEMENT
- /// ABANDON
- ⊙ EX. TREE
- ⊙ TREE PROTECTION
- ⊙ EX. PARKING METER AND PAVEMENT MARKING

EAST END WATER SYSTEM IMPROVEMENTS PROJECT

PROJECT AREA



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SITE LOCATION MAP

SCALE: 1" = 1000'

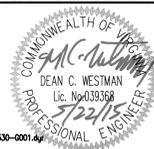
AUGUST 2015

ABBREVIATIONS

- DIP DUCTILE IRON PIPE
- EX. EXISTING
- G.V. GATE VALVE
- H. HORIZONTAL
- INV. INVERT
- MH MANHOLE
- MIN. MINIMUM
- PERM. PERMANENT
- PROP. PROPOSED
- PVC POLYVINYL CHLORIDE
- SAN. SANITARY
- TS & V TAPPING SLEEVE AND VALVE
- TYP. TYPICAL
- V. VERTICAL
- W/ WITH

WORK AREA TRAFFIC CONTROL NOTES

- ITEMS FOR TEMPORARY MAINTENANCE OF TRAFFIC SHALL BE PLACED IN ACCORDANCE WITH THE LATEST EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND THE "VIRGINIA WORK AREA PROTECTION MANUAL," DATED JAN. 1, 2003.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING HIS WORK WITH VDOT AND ANY OTHER CONTRACTORS IN THE AREA.
- ACCESS TO AND FROM ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES.
- THE CONTRACTOR SHALL COVER ANY EXISTING SIGN WHICH IS NOT APPLICABLE OR WHICH MIGHT CREATE CONFUSION TO THE MOTORIST, AS DIRECTED BY THE ENGINEER.
- ALL NON-REGULATORY AND ADVANCE WARNING SIGNS SHALL CONSIST OF BLACK LETTERS ON AN ORANGE BACKGROUND, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL MAINTAIN INTERSECTION SIGHT LINES THROUGH AREAS THAT ARE CHANNELIZED WITH DRUMS OR TEMPORARY TRAFFIC BARRIERS.
- THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF 1 LANE OF TRAFFIC AT ALL TIMES DURING WORKING HOURS AND RESTORE FULL TRAFFIC FLOW UPON COMPLETION OF WORK EACH DAY.

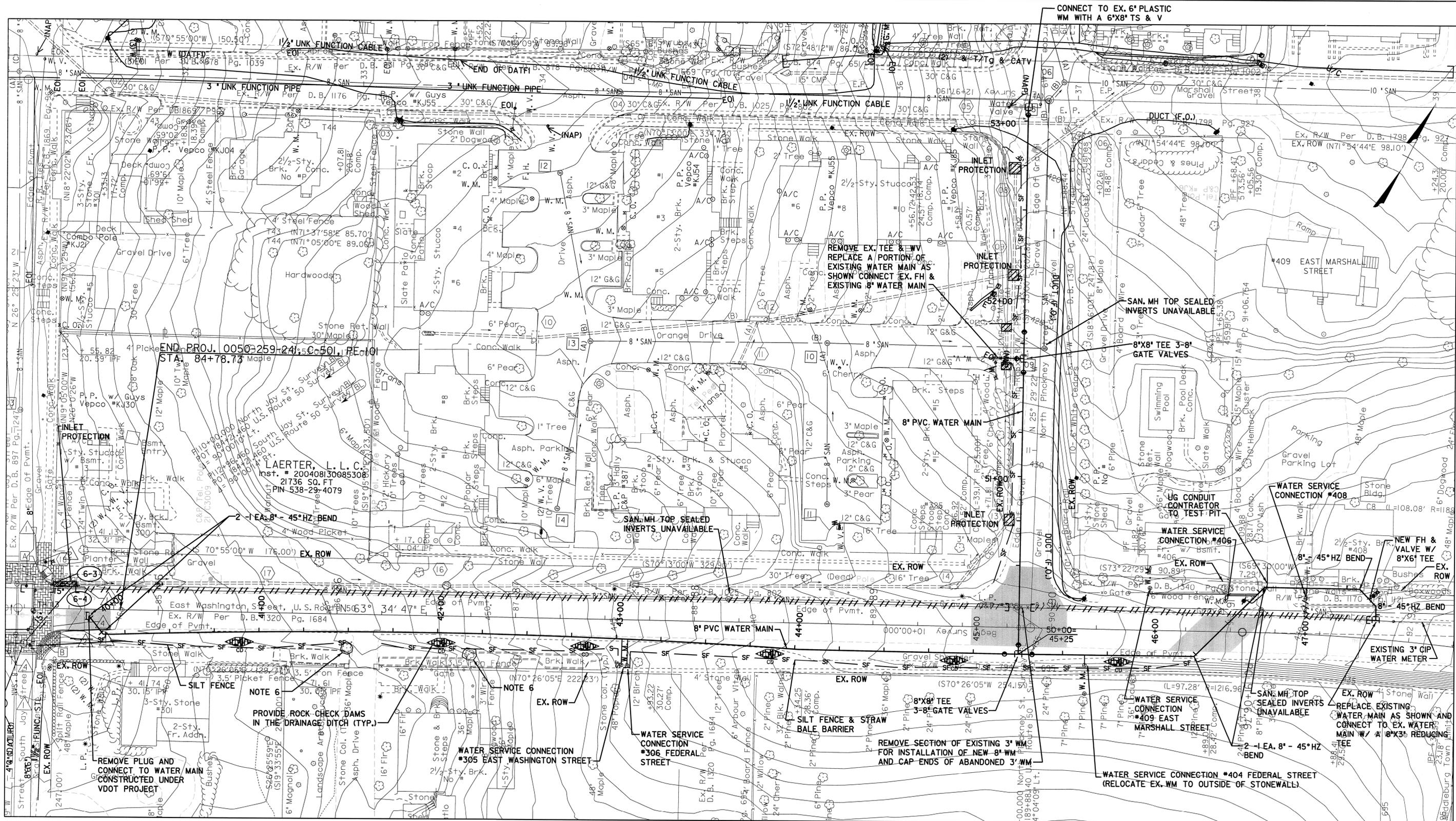


CONTRACT NO. _____

**TOWN OF MIDDLEBURG
DEPARTMENT OF UTILITIES
EAST END WATER SYSTEM IMPROVEMENT PROJECT**

REV	DATE	DESCRIPTION

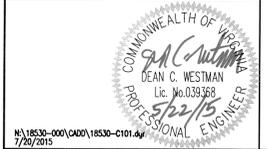
DRAWING	SHEET
COVER SHEET	G-1



ROADWAY MILLING AND PAVING NOTES:

1. THE CONTRACTOR SHALL PERFORM FULL DEPTH PAVEMENT PATCHING/RESTORATION WHERE LONGITUDINAL TRENCH OPERATIONS AND TRENCH CROSSINGS IMPACT EXISTING ROADWAYS.
2. THE CONTRACTOR SHALL REPLACE ANY PAVEMENT MARKINGS DISTURBED OR DAMAGED ON ROADWAYS.
3. THE CONTRACTOR SHALL PATCH OR REPAIR PRIVATE DRIVEWAYS AND PARKING AREAS TO MATCH EXISTING CONDITIONS, IF DISTURBED DURING CONSTRUCTION.
4. SEE SHEETS T-1 THROUGH T-6 FOR MAINTENANCE OF TRAFFIC PLANS.
5. SEE SHEET C-5 FOR PAVEMENT RESTORATION DETAIL.
6. REMOVE 2 TREES UNDER CONTINGENT ITEM C-6 IF DIRECTED BY ENGINEER.

AREA TO BE MILLED AND PAVED



WRA
 Whitman, Requardt & Associates, LLP
 3701 Pender Drive, Suite 450, Fairfax, Virginia 22030

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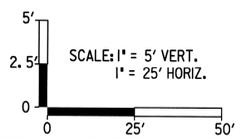
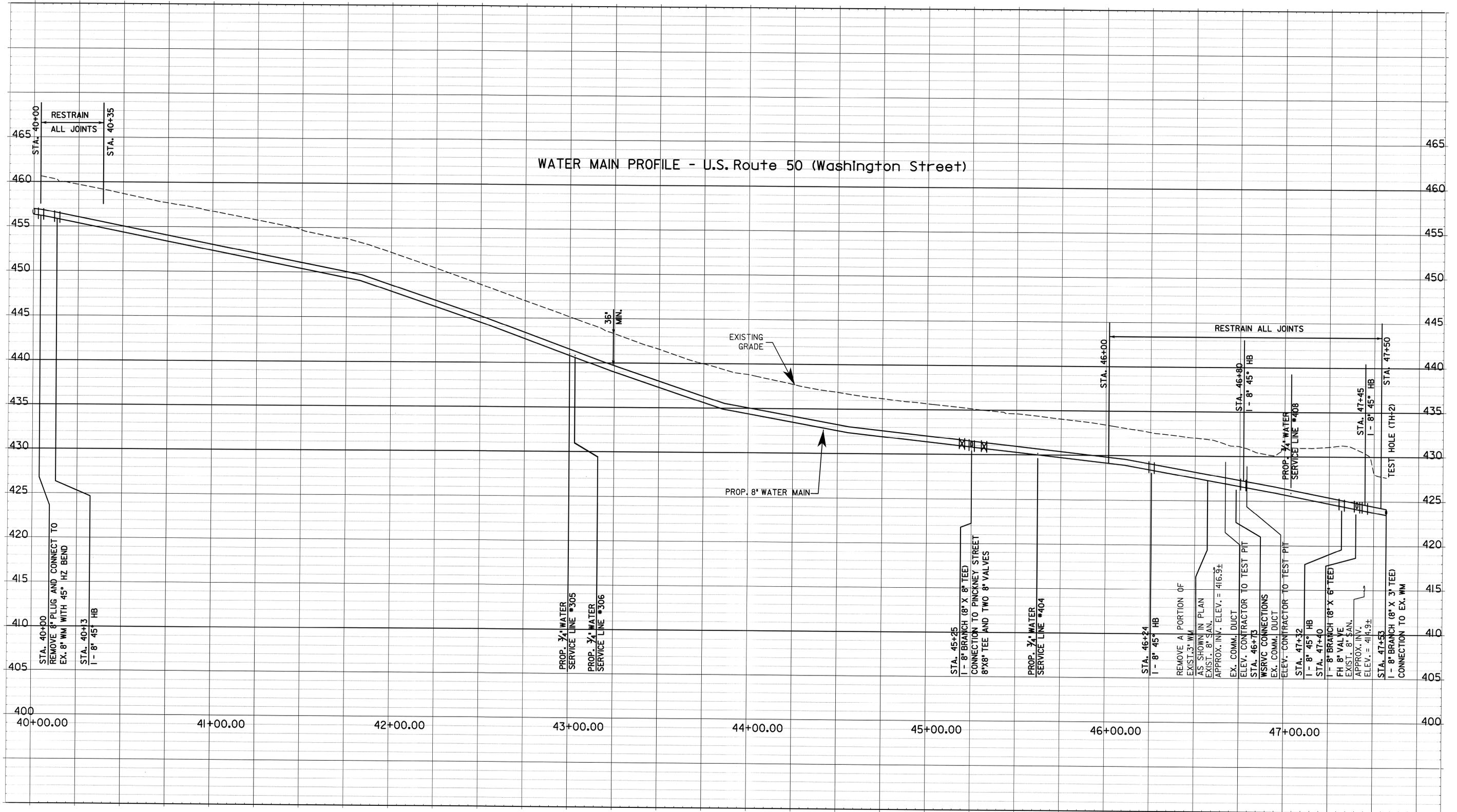
TOWN OF MIDDLEBURG
 DEPARTMENT OF UTILITIES
 EAST END WATER SYSTEM IMPROVEMENT PROJECT



REV	DATE	DESCRIPTION

DRAWING: WATER MAIN PLAN
 SHEET: C-1

WATER MAIN PROFILE - U.S. Route 50 (Washington Street)





WRA

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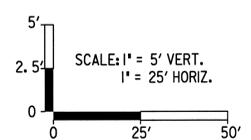
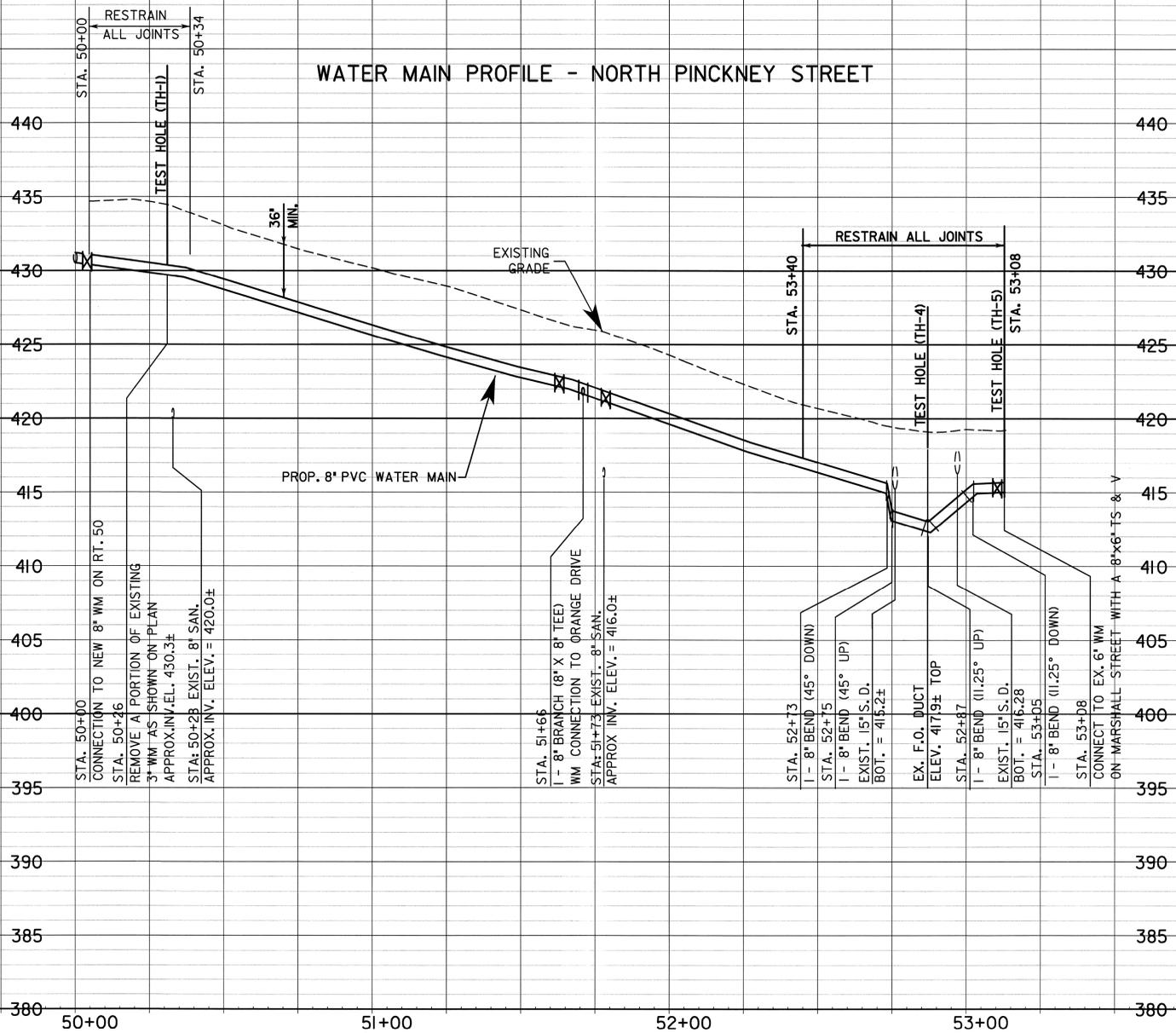
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TOWN OF MIDDLEBURG
 DEPARTMENT OF UTILITIES
 EAST END WATER SYSTEM IMPROVEMENT PROJECT

REV	DATE	DESCRIPTION
		DRAWING WATER MAIN PROFILE NORTH PINCKNEY STREET
		SHEET C-2

WATER MAIN PROFILE - NORTH PINCKNEY STREET



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 Whitman, Requardt & Associates, LLP
 3701 Pender Drive, Suite 450, Fairfax, Virginia 22030

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TOWN OF MIDDLEBURG
 DEPARTMENT OF UTILITIES
 EAST END WATER SYSTEM IMPROVEMENT PROJECT

REV	DATE	DESCRIPTION

DRAWING WATER MAIN PROFILE NORTH PINCKNEY STREET	SHEET C-3
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5/20/2015

EROSION AND SEDIMENT CONTROL

I. EROSION CONTROL NARRATIVE

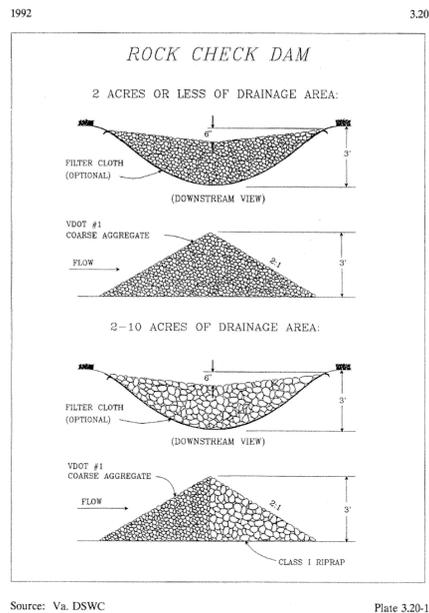
PRIOR TO ANY LAND DISTURBING OPERATIONS, THE EROSION CONTROLS, AS SPECIFIED BY THE ENGINEERING PLANS, SHALL BE INSTALLED. ALL MECHANICAL AND VEGETATIVE PRACTICES SHALL BE IN CONFORMANCE WITH THE REQUIREMENTS OF LOUDOUN COUNTY AND THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK. SLOPE AREAS THAT CANNOT BE ADEQUATELY STABILIZED BY SEEDING, SHALL BE SODDED AND STAKED. AS DISTURBED AREAS, NOT TO BE CONSTRUCTED UPON, ARE FINAL GRADED, THEY SHALL BE PREPARED LIME AND FERTILIZER APPLIED, NON-WETLAND AREAS SHALL BE SEEDING IN ACCORDANCE WITH THE SPECIFICATIONS. WETLAND AREAS SHALL BE SEEDING WITH A MIXTURE OF RED TOP, ROUGHSTALK BLUEGRASS, WILD MILLET AND SWITCHGRASS THIS RESTORATION WILL BE PERFORMED WITHIN 10 DAYS AFTER FINAL GRADING. ALL TEMPORARY SEDIMENT CONTROLS AND MECHANICAL DEVICES SHALL BE REMOVED FROM CONTRIBUTING AREAS AS THEY BECOME STABILIZED.

II. EROSION, SEDIMENTATION, AND LAND CONSERVATION NOTES

- MEASURES TO CONTROL EROSION AND SILTATION SHALL BE PROVIDED PURSUANT TO AND IN COMPLIANCE WITH CURRENT STATE AND LOCAL REGULATIONS. THE INFORMATION CONTAINED IN THE CONSTRUCTION PLANS AND/OR THE APPROVAL OF THE PLANS SHALL IN NO WAY RELIEVE THE CONTRACTOR OR HIS AGENT OF ANY LEGAL RESPONSIBILITY WHICH MAY BE REQUIRED BY THE CODE OF VIRGINIA.
- ALL AREAS, ON OR OFF-SITE, WHICH ARE DISTURBED BY THIS CONSTRUCTION AND WHICH ARE NOT PAVED OR BUILT UPON SHALL BE ADEQUATELY STABILIZED TO CONTROL EROSION AND SEDIMENTATION. ACCEPTABLE STABILIZATION SHALL CONSIST OF PERMANENT GRASS SEED MIXTURE INSTALLED IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS. ALL SLOPES 3:1 AND GREATER SHALL RECEIVE SOIL STABILIZATION IN ACCORDANCE WITH THE SPEC.
- ANY DISTURBED AREA NOT PAVED, PERMANENTLY SEEDING, SODDED, OR BUILT UPON BY 1 NOVEMBER OR DISTURBED AFTER THAT DATE, IS TO BE TEMPORARILY SEEDING WITHIN 14 DAYS WITH OATS, ABRUZZI RYE OR APPROVED EQUIVALENT, AND MULCHED WITH HAY OR STRAW MULCH AT THE RATE OF 2 TONS PER ACRE. FOR ADDITIONAL DETAILS, REFER TO THE CURRENT EDITION OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK.
- NO AREA SHALL BE DENUDED AND NOT BE DISTURBED FOR A PERIOD LONGER THAN 14 DAYS.
- TEMPORARY DIVERSIONS, SEEDING AND MULCHED OR STAKED STRAW BALE DIVERSIONS AND OTHER CONTROL MEASURES NECESSARY, ARE TO BE PLACED AS INDICATED ON THE DRAWINGS PRIOR TO OR AS THE FIRST STEP IN EXCAVATION.
- WHEN IN ACCORDANCE WITH STATE AND FEDERAL JOB SAFETY REQUIREMENTS, ALL EXCAVATED MATERIAL IS TO BE PLACED ON THE UPHILL SIDE OF TRENCHES. NO MATERIAL IS TO BE PLACED IN STREAMBEDS. ANY STOCKPILED MATERIAL WHICH WILL REMAIN IN PLACE LONGER THAN 14 DAYS IS TO BE SEEDING FOR TEMPORARY VEGETATION AND MULCHED. WHERE SOIL IS PLACED ON DOWNHILL SIDE OF TRENCH, IT IS TO BE BACK-SLOPED TO DRAIN TOWARD TRENCH. WHEN NECESSARY TO DEWATER THE TRENCH, THE PUMP DISCHARGE HOSE MUST OUTLET IN A STABILIZED AREA OR A SEDIMENT BASIN.
- WHERE STREAM CROSSINGS ARE REQUIRED FOR EQUIPMENT, TEMPORARY CULVERTS SHALL BE PROVIDED.
- NO MORE THAN 500 FEET OF TRENCH ARE TO BE OPEN AT ANY ONE TIME.
- DURING CONSTRUCTION, ALL STORM SEWER INLETS WILL BE PROTECTED BY SILT TRAPS, MAINTAINED AND MODIFIED AS REQUIRED BY CONSTRUCTION PROGRESS.
- ALL DISTURBED AREAS ARE TO BE SEEDING AND MULCHED OR SODDED WITHIN 7 DAYS AFTER BACKFILL OF THE APPLICABLE TRENCH SECTION, IN ACCORDANCE WITH THE PROVISIONS CONTAINED IN THE PROJECT SPECIFICATIONS RELATING TO SEEDING AND SODDING. SPEED IS THE ESSENTIAL LAND CONSERVATION ELEMENT FOR A LINEAR PROJECT.
- FOR FURTHER REQUIREMENTS AND DETAILS OF TREE PRESERVATION, PLANTING, EROSION AND SEDIMENT CONTROL, SEE THE VIRGINIA EROSION CONTROL HANDBOOK.

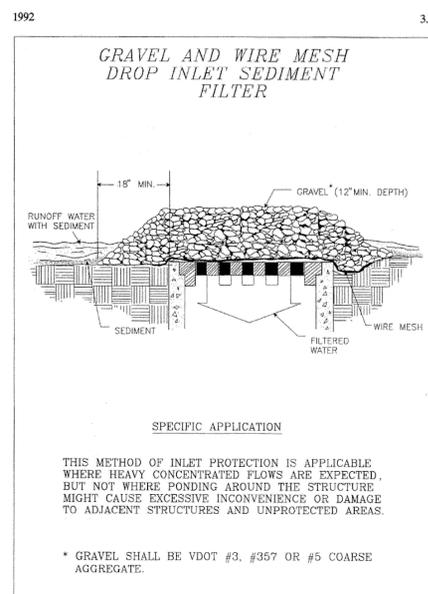
LEGEND

- | | | |
|--|--------|------------------|
| | — SF — | SILT FENCE |
| | — TP — | TREE PROTECTION |
| | | INLET PROTECTION |
| | | CHECK DAM |



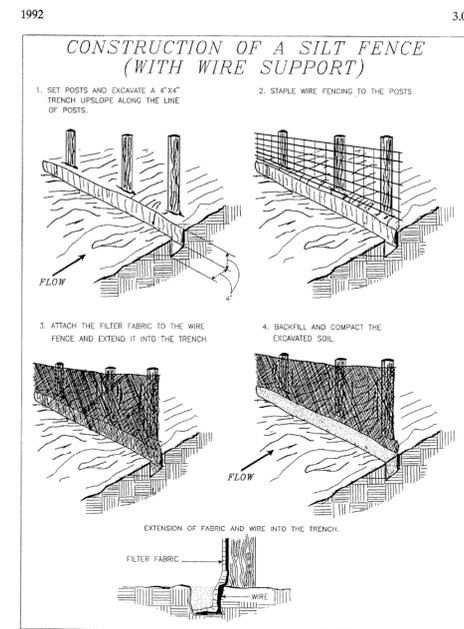
Source: Va. DSWC Plate 3.20-1

III - 187



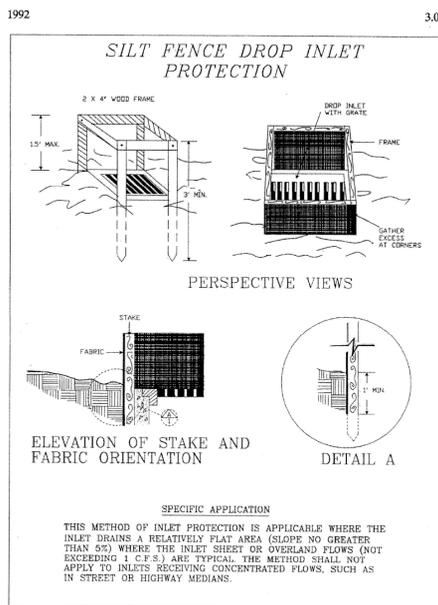
Source: Va. DSWC Plate 3.07-2

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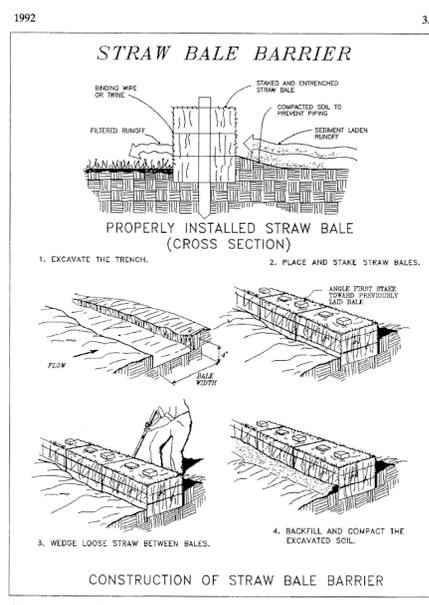
Source: Adapted from Installation of Straw and Fabric Filter Barriers for Sediment Control, Sherwood and Wyant Plate 3.05-1

III - 24



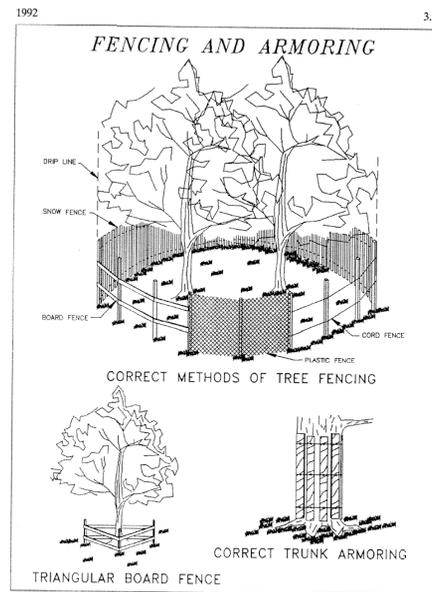
Source: N.C. Erosion and Sediment Control Planning and Design Manual, 1988 Plate 3.07-1

III - 35



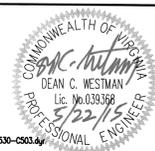
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III - 18



Source: Va. DSWC Plate 3.38-2

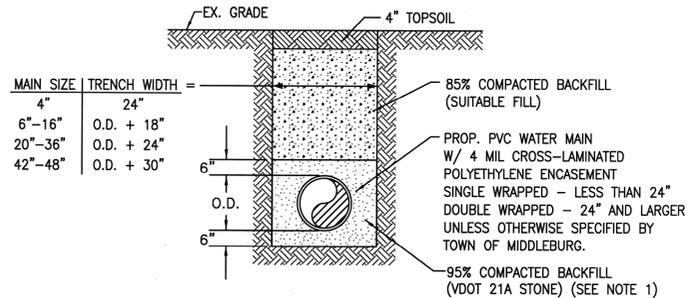
III - 401



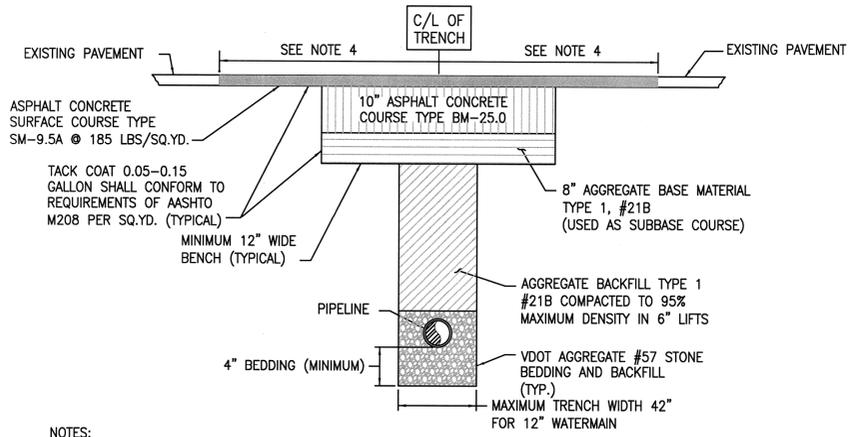
CONTRACT NO. _____

TOWN OF MIDDLEBURG
DEPARTMENT OF UTILITIES
EAST END WATER SYSTEM IMPROVEMENT PROJECT

REV	DATE	DESCRIPTION	SHEET
		DRAWING EROSION AND SEDIMENT CONTROL DETAILS	C-4

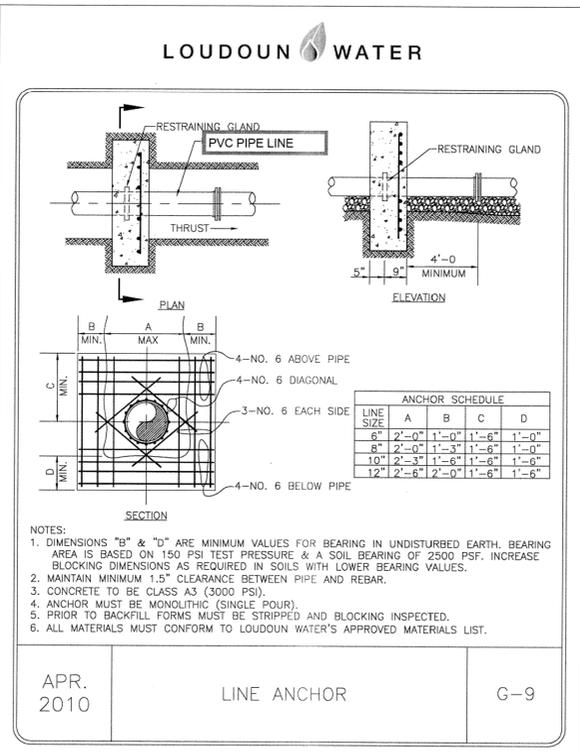


NON-PAVED AREAS RESTORATION
NOT TO SCALE



OPEN CUT PAVEMENT
RESTORATION FOR ROADWAYS
NOT TO SCALE

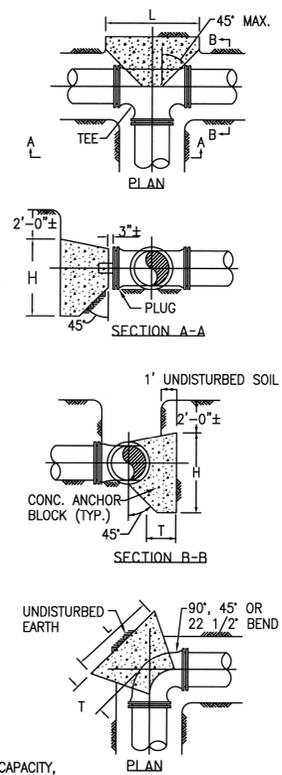
- NOTES:
1. ALL PAVEMENT MARKINGS DAMAGED OR DESTROYED BY TRENCH EXCAVATION ACTIVITY SHALL BE REPLACED BY THE PERMITEE PER VDOT SPECIFICATIONS.
 2. THIS DETAIL FOR HIGHWAYS WITH EXISTING ASPHALT CONCRETE PAVEMENT SECTIONS.
 3. THE EXTENT OF PAVEMENT RESTORATION FOR ALL PAVEMENT CUTS SHALL BE DETERMINED BY THE DISTRICT ADMINISTRATION'S DESIGNER.
 4. WIDTH OF TRAVEL LANE MINIMUM FULL PAVEMENT WIDTH FOR OPEN CUT TRENCHING ALONG ROAD CENTERLINE OR 25' (MINIMUM) FOR PERPENDICULAR CROSSROADS. FOR PINCKNEY STREET, SURFACE COURSE SHALL BE FOR TRENCH/BENCH RESTORATION WIDTH ONLY.



APR. 2010
LINE ANCHOR
G-9

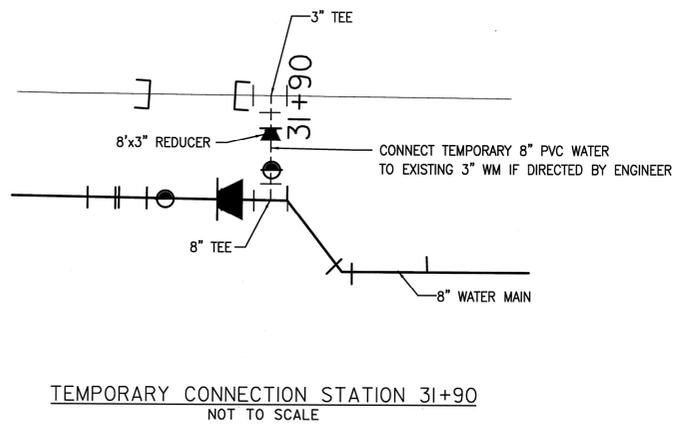
- NOTES:
1. DIMENSIONS "B" & "D" ARE MINIMUM VALUES FOR BEARING IN UNDISTURBED EARTH. BEARING AREA IS BASED ON 150 PSI TEST PRESSURE & A SOIL BEARING OF 2500 PSF. INCREASE BLOCKING DIMENSIONS AS REQUIRED IN SOILS WITH LOWER BEARING VALUES.
 2. MAINTAIN MINIMUM 1.5" CLEARANCE BETWEEN PIPE AND REBAR.
 3. CONCRETE TO BE CLASS A3 (3000 PSI).
 4. ANCHOR MUST BE MONOLITHIC (SINGLE POUR).
 5. PRIOR TO BACKFILL FORMS MUST BE STRIPPED AND BLOCKING INSPECTED.
 6. ALL MATERIALS MUST CONFORM TO LOUDOUN WATER'S APPROVED MATERIALS LIST.

PIPE SIZE INCHES	DEGREE OF BEND	(1) DESIGN PRESSURE 175 PSI		
		L	H	T
6	90	3.0	2.0	1.25
	45	2.0	1.5	1.0
	22 1/2	1.5	1.0	1.0
8	90	4.0	2.5	1.75
	45	2.5	2.0	1.0
	22 1/2	2.0	1.5	1.0
12	90	5.5	4.0	2.25
	45	4.0	3.0	1.5
	22 1/2	3.0	2.0	1.0
16	90	7.5	5.0	3.25
	45	5.0	4.5	2.0
	11 1/4 / 22 1/2	4.0	3.0	1.5
20	90	9.5	6.0	4.0
	45	7.0	4.5	2.75
	11 1/4 / 22 1/2	5.5	3.0	2.0
24	90	13.0	6.5	5.5
	45	9.0	5.0	3.5
	11 1/4 / 22 1/2	6.0	4.0	2.0

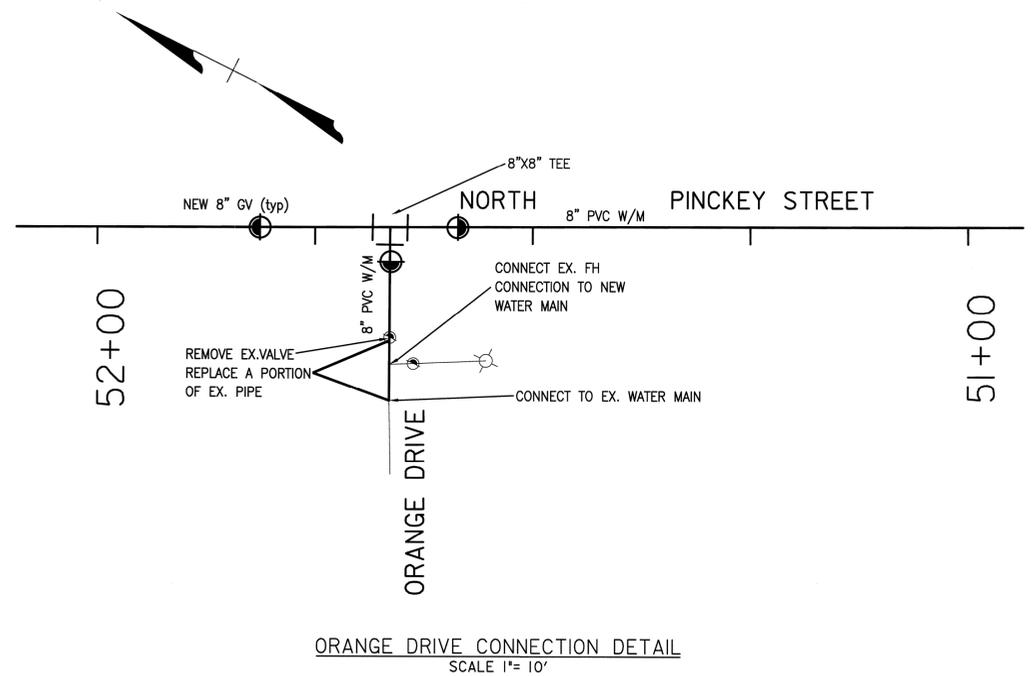


- NOTES:
1. MINIMUM CONCRETE ANCHOR BLOCK DIMENSIONS IN FEET.
 2. PROVIDE FORM WORK FOR ALL CONCRETE.
 3. CONCRETE SHALL BE CLASS B2 2000 PSI.
 4. THE ABOVE TABLE IS BASED ON 2000 PSF SOIL BEARING CAPACITY, R=2PA SIN $\theta/2$ AND FOR A TEST PRESSURE = 1.5 x WORKING PRESSURE.
 5. ANCHOR BLOCK DESIGN FOR PIPE LARGER THAN 24" SHALL BE REVIEWED ON AN INDIVIDUAL BASIS BY TOWN OF MIDDLEBURG.
 6. WRAP FITTING WITH POLYETHYLENE SHEETING. CONCRETE MUST NOT OBSTRUCT ACCESS TO MECHANICAL JOINT ASSEMBLY.
 7. CONCRETE ANCHOR BLOCK DIMENSIONS FOR TEES TO BE SAME AS FOR 90° BENDS.
 8. HEIGHT OF CONCRETE ANCHOR BLOCK ABOVE PIPE CENTERLINE IS 1/3 THE H DIMENSION.
 9. BLOCKING SHALL BACK TO UNDISTURBED EARTH.

HORIZONTAL CONCRETE ANCHORS - DETAIL
NOT TO SCALE



TEMPORARY CONNECTION STATION 31+90
NOT TO SCALE



ORANGE DRIVE CONNECTION DETAIL
SCALE 1"= 10'

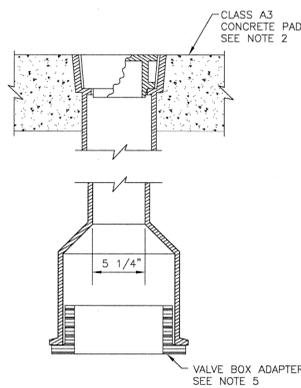


CONTRACT NO. _____

TOWN OF MIDDLEBURG
DEPARTMENT OF UTILITIES
EAST END WATER SYSTEM IMPROVEMENT PROJECT

REV	DATE	DESCRIPTION

DRAWING CIVIL AND WATER DETAILS SHEET C-5

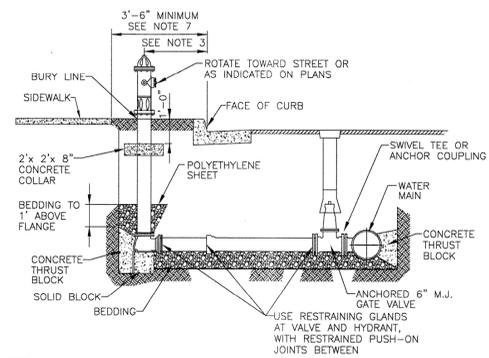


- NOTES:
1. PROVIDE EXTENSION OF VALVE STEM WHERE OPERATING NUT IS MORE THAN 5' BELOW GRADE.
 2. VALVE BOXES OUTSIDE OF PAVEMENT REQUIRE 18"x18"x6" CONCRETE PAD.
 3. REFER TO THE APPROVED MATERIALS LIST FOR APPROVED MANUFACTURERS & PART NUMBERS.
 4. ADJUSTMENT BY RISER RINGS PROHIBITED.
 5. FOR GATE VALVE INSTALLATIONS, PROVIDE VALVE BOX ADAPTER MADE TO FIT SPECIFIC SIZE AND MANUFACTURE OF VALVE. NOT TO BE USED WITH BUTTERFLY VALVES.

APRIL 2013

VALVE BOX

W-1

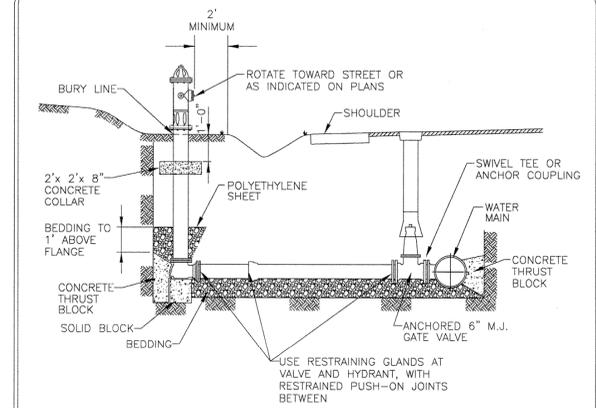


- NOTES:
1. TANGENT TEE REQUIRED WHERE SPECIFIED ON CONSTRUCTION PLANS.
 2. MAXIMUM HYDRANT HEIGHT IS 7' BURY. FOR DEEPER MAIN, PROVIDE VERTICAL OFFSET IN LEAD.
 3. MINIMUM 2' FROM FACE OF CURB TO CENTER OF HYDRANT.
 4. CONCRETE & POLYETHYLENE MUST BE CLEAR OF WEEP HOLES.
 5. VERTICAL ADJUSTMENT OF BARREL LIMITED TO ONE EXTENSION. REPLACE ENTIRE STEM WHERE BARREL IS ADJUSTED.
 6. FIELD PAINTING REQUIRED PRIOR TO ACCEPTANCE.
 7. WHERE LESS THAN 3'-6" IS AVAILABLE BETWEEN FACE OF CURB AND SIDEWALK, CENTER HYDRANT AT LEAST 2' BEHIND SIDEWALK.
 8. ALONG STREETS OF DESIGN SPEED 45 MPH AND ABOVE, LOCATE HYDRANT A MINIMUM OF 6' FROM EDGE OF PAVEMENT OR FACE OF CURB.
 9. HYDRANT MUST BE AT LEAST 5' FROM DRIVEWAY ENTRANCES.
 10. LANDSCAPE PLANTINGS ARE PROHIBITED WITHIN 5' OF HYDRANT.

OCT. 2013

FIRE HYDRANT ALONG STREET

W-10

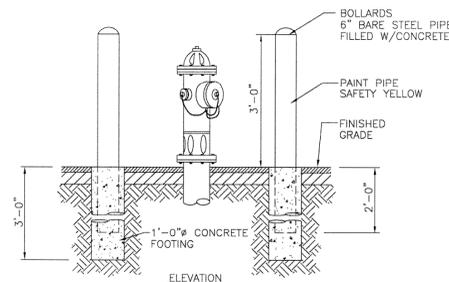
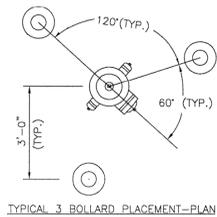


- NOTES:
1. TANGENT TEE REQUIRED WHERE SPECIFIED ON CONSTRUCTION PLANS.
 2. MAXIMUM HYDRANT HEIGHT IS 7' BURY. FOR DEEPER MAIN, PROVIDE VERTICAL OFFSET IN LEAD.
 3. CONCRETE & POLYETHYLENE MUST BE CLEAR OF WEEP HOLES.
 4. VERTICAL ADJUSTMENT OF BARREL LIMITED TO ONE EXTENSION. REPLACE ENTIRE STEM WHERE BARREL IS ADJUSTED.
 5. FIELD PAINTING REQUIRED PRIOR TO ACCEPTANCE.
 6. LOCATE HYDRANT AT LEAST 2' BEHIND DITCH. GRADE TO A MAXIMUM 10% SLOPE WITHIN 2' OF HYDRANT. VALVE MUST NOT BE IN DITCH OR SHOULDER.
 7. HYDRANT MUST BE AT LEAST 5' FROM DRIVEWAY ENTRANCES.
 8. LANDSCAPE PLANTINGS ARE PROHIBITED WITHIN 5' OF HYDRANT.

OCT. 2013

FIRE HYDRANT BEHIND DITCH

W-11

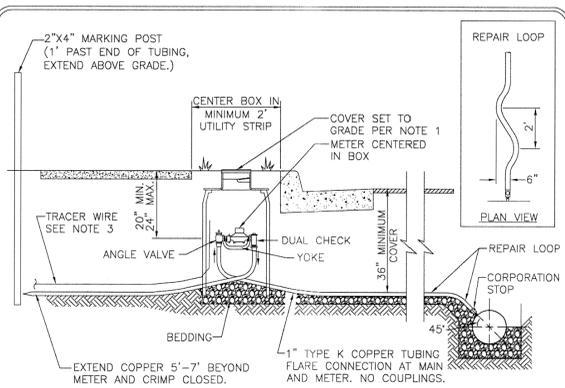


- NOTES:
1. QUANTITY & PLACEMENT OF BOLLARDS TO BE DETERMINED BY PLANS OR FIELD INSPECTION AS WARRANTED.
 2. CONCRETE TO BE CLASS A3.

APR. 2010

HYDRANT BOLLARDS

W-13



- NOTES:
1. A) AT INITIAL INSTALLATION, ROTATIONAL (ADJUSTABLE) FRAME TO BE IN CENTER OF HEIGHT RANGE.
 - B) WHERE AT CURB, SET COVER FLUSH TO 1" ABOVE TOP OF CURB.
 - C) WHERE NO CURB, SET COVER 1" ABOVE FINAL GRADE.
 - D) IN SLOPE, PROVIDE TIMBER RETAINING WALL 3' FROM COVER.
 2. WHERE TAP IS ON 4" MAIN, CORPORATION STOP SHALL HAVE 3/4" INLET AND 1" FLARE OUTLET. ON 3" WATER MAIN, USE 3"x3/4" BRASS SADDLE.
 3. WHERE SERVICE BETWEEN METER AND BUILDING IS PLASTIC TUBING, PROVIDE TRACER WIRE OF AWG #12 SOLID COPPER WITH 45 MIL POLYETHYLENE INSULATION. TURN TRACER WIRE UP INSIDE METER BOX. TERMINATE TRACER WIRE INDOORS OR AT EXTERIOR WALL WITH A BLUE SOIL MARKER.
 4. ALL MATERIALS MUST CONFORM TO LOUDOUN WATER'S APPROVED MATERIALS LIST.
 5. METER FURNISHED AND INSTALLED BY LOUDOUN WATER.

APR. 2013

SERVICE CONNECTION FOR 3/4" AND 1" METERS

W-23



Whitman, Requardt & Associates, LLP
3701 Pender Drive, Suite 450, Fairfax, Virginia 22030

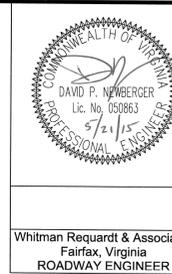
CONTRACT NO. _____

TOWN OF MIDDLEBURG
DEPARTMENT OF UTILITIES
EAST END WATER SYSTEM IMPROVEMENT PROJECT

REV	DATE	DESCRIPTION

DRAWING CIVIL AND WATER DETAILS SHEET C-6

PROJECT MANAGER: MARTHA SEMMES (540) 687-5152
SURVEYED BY, DATE: RICE ASSOCIATES (703) 968-3200
DESIGN BY: WHITMAN, REQUARDT & ASSOCIATES, LLP (703) 293-9172
SUBSURFACE UTILITY BY, DATE: ACCUMARK (703) 635-3074



REVISED	STATE	STATE	SHEET NO
	VA.	50	T-1

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

Whitman Requardt & Associates
Fairfax, Virginia
ROADWAY ENGINEER

TOWN OF MIDDLEBURG DEPARTMENT OF UTILITIES EAST END WATER SYSTEM IMPROVEMENT PROJECT TRAFFIC MANAGEMENT PLAN

Public Communications Plan
VDOT Northern Virginia District
Project Number:
UPC Number:
Route Number: <50> Town of Middleburg

Project Description:
The purpose of the project is to install approximately 1,150' of water line along Route 50 from N. Jay Street to East of N. Pinckney Street and along N. Pinckney Street from E. Marshall Street to Route 50. Work is anticipated to be performed in 100 foot segments. At the end of day, the contractor is responsible for maintaining a 6:1 wedge and protecting the work area from traffic.

Traffic Impacts: During construction, motorists should watch for flaggers, temporary signals, signs, and changing traffic patterns while crews install the water line. Traffic will travel on existing pavement with alternating lane closures.

Crews are expected to begin work on the project in June 2016 and the work is expected to continue for one month.

Crisis Communications Plan:

- As with any crisis, emergency responders (911) should be notified immediately if necessary.
- The Area Construction Engineer (ACE) 757-925-2500 or his designee should be notified immediately.
- If the emergency is traffic related, the ACE or his designee should immediately notify the Traffic Operations Center.
- The Traffic Operations Center should immediately notify Public Affairs.
- The ACE, public affairs, and Traffic Operations Center will work together to inform the traveling public, emergency responders and the media about delays and unexpected changes in traffic patterns using the contact list below, and other resources if necessary.

Regional Operations:

Will work with the project team to ensure that local media, Virginias 511, and other information dissemination methods are utilized to keep Virginias travelers informed of activities on this project. We will also monitor adjacent work activities to ensure minimal impact to the project. Detour routes will be monitored and project staff informed of potential impacts due to incidents on these routes.

The Traffic Operations Center (TOC) will serve as a one point contact source for event notification. The TOC will contact appropriate individuals based on the severity of the event. All project staff will be given business cards with TOC contact information contained within this document.

- Incident/ Emergency Management Plan**
As with any incident requiring emergency responders, the person identifying the incident shall notify 911 immediately.

After 911 contact is made, or if the event does not require emergency response then contact the TOC at (703) 877-3450.

When an incident occurs, the following information needs to be compiled in order to get information disseminated with the appropriate individuals.

- What**
Incident/Roadwork or closure description
- Where**
Location of event
Lanes affected
- Effect**
Effect in travel
Audience for action
Action for audience to take
Reasons for action

Clearance of incidents shall be accomplished using the Incident Command System and National Incident Management System (NIMS) principals. Due to the critical nature of this project, all project staff should have completed the IS-700 National Incident Management System (NIMS), "An Introduction". The Virginia State Police maintain a list of local towing and recovery operations in the area. Should towing and recovery operations be required, project staff shall work with the Regional Incident Management Coordinator and the Va. State Police in coordinating efforts.

The TOC will immediately notify Public Affairs when an event occurs.

Public affairs and the Traffic Operations Center will work together to inform the traveling public, emergency responders, the media and VDOT executive staff about delays and unexpected changes in traffic patterns using the contact list below, and other resources if necessary.

Traffic Backup Notification

The Traffic Operations Center shall be notified of major traffic backups related to the work by the project staff. The TOC will make all entries into systems that feed this information into 511. The TOC will also make notifications to department staff. The staff notifications shall include but not be limited to the Area Construction Engineer, Project Maint. of Traffic Coordinator, Project Manager, Resident Administrator, District Work Zone Safety Coordinator, Regional Traffic Engineer, Regional Traffic Operations Manager, Regional Incident Management Coordinator, and Public Affairs Manager of any incidents and expected traffic delays.

Emergency contact for this project shall be 911. In the event that other notifications are needed, the following list shall be populated at the construction kick-off meeting:

	NAME	Telephone
	Area Construction Engineer	
	Project Maint. of Traffic Coordinator	
	Project Manager	
	Resident Administrator	
	District Work Zone Safety Coordinator	
	Regional Traffic Engineer	
	Regional Traffic Operations Manager	
	Regional Incident Management Coordinator	

A review of all major incidents, as determined by the Regional Incident Management Coordinator, shall be accomplished within 72 hours of clearance of the incident. VDOT Project staff, Regional Operations Staff, Contractor Staff, and emergency responders shall be represented at these meetings. The purpose of this review is to implement lessons learned from incidents.

Periodic reviews with VDOT Project Staff, Regional Operations Staff, and Contractor Staff shall be conducted throughout the life of the project to identify potential problems.

Traffic Management Plan/ Temporary Traffic Control Plan

- Project is Category II
- Traffic consists of Commuters, some Residential, Police/Fire & Rescue and Trucks.
- No work restricting two-way traffic movement will be allowed between the hours of 5:30am to 9:00 am and 3:00pm to 6:30pm.
- WORK ZONE:**
Route 50:
Sta. 84+50 to Sta. 92+00
Width - 10' - 15'
N. Pinckney Road:
Sta. 10+00 to 13+00
Width - 10' - 15'
- Holiday restrictions outlined in 2007 R&B specifications.
Lane closures will not be permitted from 12:00 noon the day before a holiday until 12:00 noon the day after a holiday. If a holiday occurs Sunday, then from 12:00 noon Friday until 12:00 noon Tuesday will be applicable, unless otherwise directed by the Engineer.
- The Contractor shall maintain 1 lane with minimum 11 feet wide during construction unless otherwise directed by the Engineer.
- The Contractor will be responsible for securing staging areas for equipment and materials.
- All traffic control devices shall be placed in accordance with the standards contained in the Virginia W.A.P.M. (2011) and the M.J.T.C.D. (2009)
- Provisions for flagger control will be made in accordance with page 6E-14 (Table 6E-1) of the Work Area Protection Manual.
- Entrances, Intersections, and Pedestrian access points that will be affected during construction:
7 Private entrances
3 Secondary Roads
- For maintenance of traffic "General Notes" see sheet number T(2).

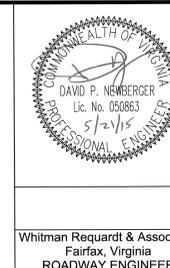
CONTACTS:

Target Audience	Contact Method	Ph. #, e-mail or address	Responsible Charge
Motorists	News Releases, weekly traffic information (Roadwatch), 511, VMS	See Public Affairs news release distribution list (e-mail and fax)	District Public Affairs Office Lauren Hansen 757-925-1660, 757-925-2584
Hospitals Fire EMS Police Department			Area Construction Engineer Project Inspector
Residential neighborhoods			
County/Town Officials			Area Construction Engineer
Businesses: n/a	Phones and Fliers	Telephone	ACE or Inspector
Schools: Middleburg Elementary School The Hill School	Phone	Main Number: (540) 751-2490 (540) 687-5897	Residing Principals: Shawn Lyons Trevor Lord

PROJECT	SHEET NO
	T-1

PROJECT MANAGER: MARTHA SEMMES (540) 687-5152
SURVEYED BY, DATE: RICE ASSOCIATES (703) 968-3200
DESIGN BY: WHITMAN, REQUARDT & ASSOCIATES, L.P. (703) 293-9171
SUBSURFACE UTILITY BY, DATE: ACCUMARK (703) 635-3074

SEQUENCE OF CONSTRUCTION & TRANSPORTATION MANAGEMENT PLANS



Whitman Requardt & Associates
Fairfax, Virginia
ROADWAY ENGINEER

REVISED	STATE		PROJECT	SHEET NO.
	STATE	ROUTE		
	VA.	50		T-2

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

GENERAL NOTES

- THE TMP FOR THIS PROJECT IS CATEGORIZED AS TYPE A, CATEGORY II.
- THE CONTRACTOR SHALL PLAN AND EXECUTE THE WORK IN ACCORDANCE WITH THE SEQUENCE OF CONSTRUCTION PLAN UNLESS A CHANGE IS APPROVED OR DIRECTED BY THE ENGINEER AND/OR VDOT INSPECTOR. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE VDOT ROAD AND BRIDGE SPECIFICATIONS DATED 2007, MUTCD 2009 EDITION, THE 2011 VIRGINIA SUPPLEMENT TO THE MUTCD, THE 2011 VIRGINIA WORK AREA PROTECTION MANUAL, AND AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR IS EXPECTED TO FOLLOW THE TRAFFIC CONTROL PLANS AS PROPOSED. IF THE CONTRACTOR WISHES TO DEVIATE FROM THE PROPOSED TRAFFIC CONTROL PLANS, THE CONTRACTOR SHALL DEVELOP PLANS THAT CONFORM TO THE 2011 VIRGINIA WORK AREA PROTECTION MANUAL, MUTCD, 2009 EDITION, VDOT'S IIM-LD-241, AND VDOT'S 2007 ROAD AND BRIDGE SPECIFICATIONS. THE CONTRACTOR SHALL SUBMIT THE TRAFFIC CONTROL PLAN FOR APPROVAL BY THE ENGINEER PRIOR TO THE INSTALLATION OF ANY TEMPORARY LANE CLOSURE MEASURES. THE CONTRACTOR WILL DEVELOP THE TRAFFIC CONTROL PLAN AT NO COST TO THE PROJECT. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE FOR SAFE TRAVEL ON THE ROADWAYS.
- THE TRANSPORTATION MANAGEMENT AND TEMPORARY TRAFFIC CONTROL PLANS ARE INTENDED AS A GUIDE. THEY ARE NOT TO ENUMERATE EVERY DETAIL WHICH MUST BE CONSIDERED IN THE CONSTRUCTION OF EACH STAGE BUT ONLY TO SHOW GENERAL HANDLING OF TRAFFIC.
- AT THE CONCLUSION OF EACH WORKDAY, ALL AREAS EXCAVATED BELOW THE EXISTING PAVEMENT SURFACE AND WITHIN THE CLEAR ZONE, SHALL BE BACKFILLED WITH APPROVED MATERIAL TO FORM AN APPROXIMATE 6:1 WEDGE AGAINST THE EXISTING PAVEMENT SURFACE FOR THE SAFETY AND PROTECTION OF VEHICULAR TRAFFIC. ALL COSTS FOR PLACING, MAINTAINING, AND REMOVING THE 6:1 WEDGE SHALL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS IN THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. SEE WEDGE DETAIL, THIS SHEET.
- EXISTING SURFACE, AGGREGATE BASE AND SUBBASE MATERIAL WHICH WILL BE DEMOLISHED OR OBLITERATED DURING CONSTRUCTION AND WHICH IS SUITABLE FOR MAINTENANCE OF TRAFFIC AS DETERMINED BY THE ENGINEER SHALL BE SALVAGED AND UTILIZED FOR MAINTENANCE OF TRAFFIC PRIOR TO THE USE OF COMMERCIAL MATERIALS. ALL COSTS ASSOCIATED WITH THE REUSE OF THESE SALVAGED MATERIALS FOR MAINTENANCE OF TRAFFIC INCLUDING DOUBLE HANDLING, HAULING AND DISPOSAL OF UNUSED MATERIALS SHALL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS IN THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- PEAK HOURS: MON-FRI 5:30 AM - 9:00 AM AND 3:00 PM - 6:30 PM
NON-PEAK HOURS: MON-FRI 9:00 AM - 3:00 PM, 6:30 PM - 5:30 AM AND SAT-SUN ALL DAY
LANE CLOSURES AND FLAGGING OPERATIONS WILL NOT BE PERMITTED DURING PEAK HOURS UNLESS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN FOR APPROVAL BY THE ENGINEER PRIOR TO THE INSTALLATION OF ANY TEMPORARY LANE CLOSURE MEASURES. LANE CLOSURES THAT RESTRICT TRAFFIC WILL NOT BE PERMITTED ON HOLIDAYS FROM NOON THE DAY BEFORE A HOLIDAY UNTIL NOON THE DAY AFTER A HOLIDAY UNLESS APPROVED BY THE ENGINEER. WHEN A HOLIDAY FALLS ON A FRIDAY, LANE CLOSURES ARE NOT PERMITTED FROM NOON THURSDAY UNTIL NOON ON MONDAY. WHEN THE HOLIDAY FALLS ON MONDAY LANE CLOSURES ARE NOT PERMITTED FROM NOON FRIDAY UNTIL NOON ON TUESDAY.
- UNDER NO CIRCUMSTANCES WILL CONCURRENT CONSTRUCTION LEFT AND RIGHT OF ANY LANE BE ALLOWED. (UNLESS OTHERWISE DIRECTED BY THE ENGINEER)
- TEMPORARY LANE WIDTHS SHALL NOT BE LESS THAN 11 FEET UNLESS OTHERWISE SPECIFIED BY ENGINEER.
- THE CLEAR ZONE SHALL BE MAINTAINED FREE OF PARKED EQUIPMENT AND STORED MATERIAL OR PROTECTED AT THE END OF EACH DAY IN ACCORDANCE WITH THE WORK AREA PROTECTION MANUAL.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ANY EXISTING SIGNS UNLESS OTHERWISE ADVISED BY THE ENGINEER TO REMOVE OR RELOCATE.
- ALL TRAFFIC CONTROL DEVICES SHALL BE APPROXIMATELY PLACED AND MOVED AS NECESSARY TO MAINTAIN ADEQUATE PROPERTY OWNER ACCESS AT ALL TIMES. WORK MAY REQUIRE ADDITIONAL TRAFFIC CONTROL DEVICES, GRADING, AND TEMPORARY PAVEMENT FOR PASSAGE OF PEDESTRIANS, VEHICULAR AND EMERGENCY TRAFFIC THROUGH THE WORK AREAS BOTH DURING AND AFTER WORKING HOURS TO MAINTAIN SUCH ACCESS.
- ALL PAVEMENT MARKINGS AND TRAFFIC FLOW ARROWS ARE SCHEMATIC ONLY. THE ACTUAL LOCATION AND APPLICATION OF PAVEMENT MARKINGS SHALL BE MADE IN ACCORDANCE WITH SECTION 704 OF THE VDOT ROAD AND BRIDGE SPECIFICATIONS DATED 2007, THE MUTCD 2009 EDITION, THE 2011 VIRGINIA SUPPLEMENT TO THE MUTCD, THE 2008 ROAD AND BRIDGE STANDARDS, OR AS
- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING THE CONSTRUCTION, SIGNING AND TRAFFIC MANAGEMENT PLAN WITH OTHER ADJACENT PROJECTS UNDER CONSTRUCTION.
- ALL EROSION AND SEDIMENT CONTROL MEASURES AND TEMPORARY DRAINAGE SHALL BE IN PLACE PRIOR TO THE BEGINNING OF CONSTRUCTION.
- IF TRAFFIC BARRIER SERVICE IS UTILIZED, IT SHALL BE INSTALLED AND REMOVED SO AS NOT TO PRESENT ANY BLUNT END OR HAZARD TO THE MOTORING PUBLIC. THE PLACEMENT AND REMOVAL OF THE TRAFFIC BARRIER SERVICE AND BARRICADES ARE TO BE COORDINATED BY THE PROJECT SAFETY OFFICER.
- PRIOR TO THE PLACEMENT OF ANY TEMPORARY BARRIER SERVICE WHERE THE PAVEMENT WILL BE SAW CUT AND NEW PAVEMENT PLACED, THE CONTRACTOR SHALL VERIFY THE EDGE OF THE FULL DEPTH PAVEMENT.
- ALL PERMANENT PAVEMENT IS TO BE CONSTRUCTED THROUGH THE INTERMEDIATE COURSE ONLY IN THE INITIAL STAGES. THE FINAL SURFACE COURSE IS TO BE APPLIED DURING THE FINAL STAGE WHEN APPROVED BY THE ENGINEER, DIRECTED BY THE ENGINEER.

- ACCESS TO ADJACENT RESIDENTIAL AND COMMERCIAL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY EACH AFFECTED PROPERTY OWNER AT LEAST 24 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OF ACCESS.
- WHEN PROCEEDING FROM ONE STAGE OF CONSTRUCTION TO ANOTHER STAGE OF CONSTRUCTION ANY EXISTING OR CONSTRUCTION PAVEMENT MARKINGS THAT DO NOT ALIGN WITH THE NEW TRAFFIC PATTERNS AND/OR NECESSARY MARKINGS SHALL BE ERADICATED AND RE-STRIPED.
- WORK OPERATIONS WILL RESTRICT LANE WIDTHS OR DETOUR TRAFFIC SHALL NOT BE INITIATED UNTIL THE ENGINEER HAS NOTIFIED DMV OF THE WORK OPERATION AND LOCATION IN ORDER TO PROPERLY NOTIFY AND RE-ROUTE OVERSIZED TRAFFIC.
- PRIOR TO CLOSING LANES OF A ROADWAY OR DETOURING TRAFFIC, LOCAL FIRE, RESCUE AND LAW ENFORCEMENT SERVICES CANNOT BE OBTAINED THE CONTRACTOR SHALL MAKE ACCOMMODATIONS TO ROUTE EMERGENCY VEHICLES SAFELY THROUGH THE WORK ZONE UNDER APPROVAL AND DIRECTION OF THE ENGINEER.
- SIDEWALKS AND PEDESTRIAN ACCESS SHALL BE MAINTAINED DURING CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.
- "ROAD WORK AHEAD," "END ROAD WORK," AND "SIDEWALKS CLOSED" SIGNS FOR WASHINGTON STREET AND SIDE ROADS SHALL REMAIN IN PLACE FOR THE ENTIRE PROJECT DURATION.
- VDOT HAZMAT MANAGER OR ASSISTANT HAZMAT MANAGER SHALL BE ON SITE FOR ANY WORK ON AREAS THAT ENCOUNTER HAZMAT MATERIALS. NO HAZMAT AREAS HAVE BEEN IDENTIFIED AT THIS TIME.
- TEMPORARY AGGREGATE DRAINAGE BACKFILL - IN ORDER TO ACCOMMODATE AND ECONOMIZE EFFICIENT USE OF ASPHALT EQUIPMENT AND PERSONNEL WHILE PATCHING THE EXISTING ROADWAY AFTER INSTALLING DRAINAGE PIPE OR CULVERT THE CONTRACTOR WILL BE PERMITTED TO INSTALL NO. 21B AGGREGATE MATERIAL AS A TEMPORARY BACKFILL MEASURE TO BRING THE PIIP TRENCH TO SUITABLE GRADE TO PERMIT OPENING THE ROADWAY TO TRAFFIC UNTIL PERMANENT ASPHALT PATCHING MATERIAL IS INSTALLED. TEMPORARY AGGREGATE DRAINAGE BACK FILL WILL ONLY BE PERMITTED FOR A PERIOD NOT TO EXCEED 24 HOURS AFTER THE PIPE OR CULVERT IS INSTALLED IN THE TRENCH. PIPE INSTALLATION SHALL BE IN ACCORDANCE WITH SECTION 302 OF THE SPECIFICATIONS. TEMPORARY AGGREGATE DRAINAGE BACKFILL WILL NOT BE MEASURED FOR SEPARATE PAYMENT BUT THE COST THEREOF SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND CONSIDERED INCIDENTAL TO THE PIPE INSTALLATION.
- TEMPORARY SHORING MAY BE REQUIRED TO MAINTAIN TRAFFIC DURING THE CONSTRUCTION OF DRAINAGE STRUCTURES AT VARIOUS LOCATIONS. THE COSTS FOR TEMPORARY SHORING SHALL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS.
- THE CONTRACTOR SHALL NOT DISTURB OR REMOVE ANY TRAFFIC CONTROL SIGNS OR REMOVE ANY OTHER TRAFFIC CONTROL DEVICES WITHOUT PRIOR PERMISSION FROM VDOT. ANY TRAFFIC CONTROL SIGNS, CONDUIT, CABLE OR OTHER TRAFFIC CONTROL DEVICES DISTURBED OR DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO VDOT OR MUNICIPALITY.

PUBLIC COMMUNICATIONS PLAN

THE PUBLIC SHALL BE NOTIFIED OF THE EXPECTED SCHEDULE ON VDOT'S WEB SITE FOR THIS PROJECT. INFORMATION OF THE POTENTIAL FOR BACK-UPS DURING THE PEAK HOURS OF OPERATION IS PROVIDED BY THE REGIONAL TRAFFIC OPERATIONS CENTER (TOC).

TRANSPORTATION OPERATIONS PLAN

THE PUBLIC AFFAIRS SECTION AND THE TRAFFIC OPERATIONS CENTER SHALL BE NOTIFIED BY THE CONSTRUCTION PROJECT MANAGER OF LANE CLOSURE INFORMATION FOR DISTRIBUTION ON THE 511 SYSTEM AND VOIS. THE CONSTRUCTION PROJECT MANAGER SHALL BE NOTIFIED ONE WEEK IN ADVANCE OF LANE CLOSURES. EMERGENCY RESPONSE PROFESSIONALS SHALL RESPOND TO TRAFFIC INCIDENTS IN THE WORK ZONE AS SOON AS POSSIBLE. BY NOON ON EACH WEDNESDAY, THE CONTRACTOR WILL SUBMIT TO THE CONSTRUCTION PROJECT MANAGER IN WRITING, A REQUEST FOR LANE CLOSURES FOR THE FOLLOWING WEEK.

THE FOLLOWING IS THE CONTACT LIST OF EMERGENCY RESPONSE AGENCIES IN CASE AN INCIDENT OCCURS IN THE WORK ZONE:

POLICE/AMBULANCE/FIRE SAFETY /HAZMAT SPILLS - 911
VDOT TRAFFIC OPERATIONS CENTER - (703) 877-3401
VIRGINIA STATE POLICE - (703) 791-3701
VDOT NOVA DISTRICT - (703) 259-2944 (8:15 AM - 4:00 PM)
TOWN OF MIDDLEBURG, TOWN ADMINISTRATOR - (540) 687-5152

FOLLOWING ANY TRAFFIC INCIDENTS, THE SITE SHALL BE CLEARED AND RESTORED FOR NORMAL TRAFFIC OPERATIONS AS SOON AS POSSIBLE.

TRAFFIC INCIDENTS WILL BE INVESTIGATED AND MEASURES INTRODUCED TO REDUCE OCCURANCES. IF NECESSARY THE MAINTENANCE OF TRAFFIC PLANS MAY BE REVISED IN CONSULTATION WITH VDOT ENGINEER.

SEQUENCE OF CONSTRUCTION

- INSTALL SIGNING FOR PROJECT LIMITS AS SHOWN IN FIGURE TTC-53.0 OF THE VIRGINIA WORK AREA PROTECTION MANUAL (WAPM).
- STAGE 1A

- SUSPEND EASTBOUND RTE. 50 ON-STREET PARKING WITHIN WORK ZONE.
- INSTALL SIGNING ON SIDE STREETS, ENTRANCES, AND APPROACHES FOR A "STREET CLOSURE OPERATION WITH DETOUR" IN ACCORDANCE WITH TTC-34.0 OF THE WAPM.
- BEGINNING WEST OF THE NORTH JAY STREET INTERSECTION, CLOSE THE EASTBOUND LANE ON EAST WASHINGTON STREET (RTE. 50). SHIFT TRAFFIC INTO TEMPORARY ONE-LANE/TWO-WAY TRAFFIC PATTERN ALONG WESTBOUND RTE. 50 IN ACCORDANCE WITH "LANE CLOSURE OPERATION ON A TWO-LANE ROADWAY USING TRAFFIC CONTROL SIGNALS" AS SHOWN ON PLAN SHEET T-3.
- EXCAVATE, DEMOLISH PAVEMENT, LAY PIPE, AND REPLACE PAVEMENT.

STAGE 1B

- INSTALL SIGNING ON SIDE STREETS, ENTRANCES, AND APPROACHES FOR A "STREET CLOSURE OPERATION WITH DETOUR" IN ACCORDANCE WITH TTC-34.0 OF THE WAPM.
- BEGINNING WEST OF THE NORTH PINCKNEY STREET INTERSECTION, CLOSE THE WESTBOUND LANE ON EAST WASHINGTON STREET (RTE. 50). SHIFT TRAFFIC INTO TEMPORARY ONE-LANE/TWO-WAY TRAFFIC PATTERN ALONG EASTBOUND RTE. 50 IN ACCORDANCE WITH "LANE CLOSURE OPERATION ON A TWO-LANE ROADWAY USING TRAFFIC CONTROL SIGNALS" AS SHOWN ON PLAN SHEET T-3.
- EXCAVATE, DEMOLITION PAVEMENT, LAY PIPE, AND REPLACE PAVEMENT.

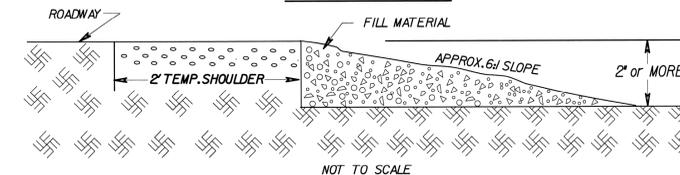
STAGE 2

- CLOSE THE SOUTHBOUND LANE ON NORTH PINCKNEY STREET BETWEEN EAST MARSHALL STREET AND EAST WASHINGTON STREET (RTE. 50). SET UP FLAGGING OPERATION IN ACCORDANCE WITH FIGURE TTC-23.0 OF THE WAPM AS SHOWN ON PLAN SHEET T-5. THE FLAGGING OPERATIONS AT THE INTERSECTION OF EAST MARSHALL STREET AND EAST WASHINGTON STREET (RTE. 50) SHALL BE IN ACCORDANCE WITH FIGURE TTC-28.0 OF THE WAPM AS SHOWN ON PLAN SHEET T-5.
- EXCAVATE, DEMOLITION PAVEMENT, LAY PIPE, AND REPLACE PAVEMENT.

MILL AND RESURFACE IN ACCORDANCE WITH FIGURE TTC-58.0 OF THE WAPM.

INSTALL FINAL ROADWAY PAVEMENT MARKINGS IN ACCORDANCE WITH FIGURE TTC-13.0 OF THE WAPM.

WEDGE DETAIL

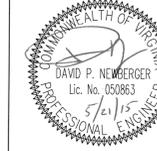


All areas excavated below pavement surfaces at the conclusion of each workday shall be backfilled with aggregate base material to form an approximate 6:1 wedge against the existing pavement surface for the safety and protection of vehicular traffic. All cost for placing, maintaining and removing 6:1 wedge shall be included in the price bid for other items in the contract and no additional compensation will be allowed.

PROJECT	SHEET NO.
	T-2

PROJECT MANAGER: MARTHA SEMMES (540) 687-5152
SURVEYED BY: DATE RICE ASSOCIATES (703) 968-3200
DESIGN BY: WHITMAN, REQUARDT & ASSOCIATES, L.P. (703) 293-9717
SUBSURFACE UTILITY BY: DATE ACCUMARK (703) 635-3074

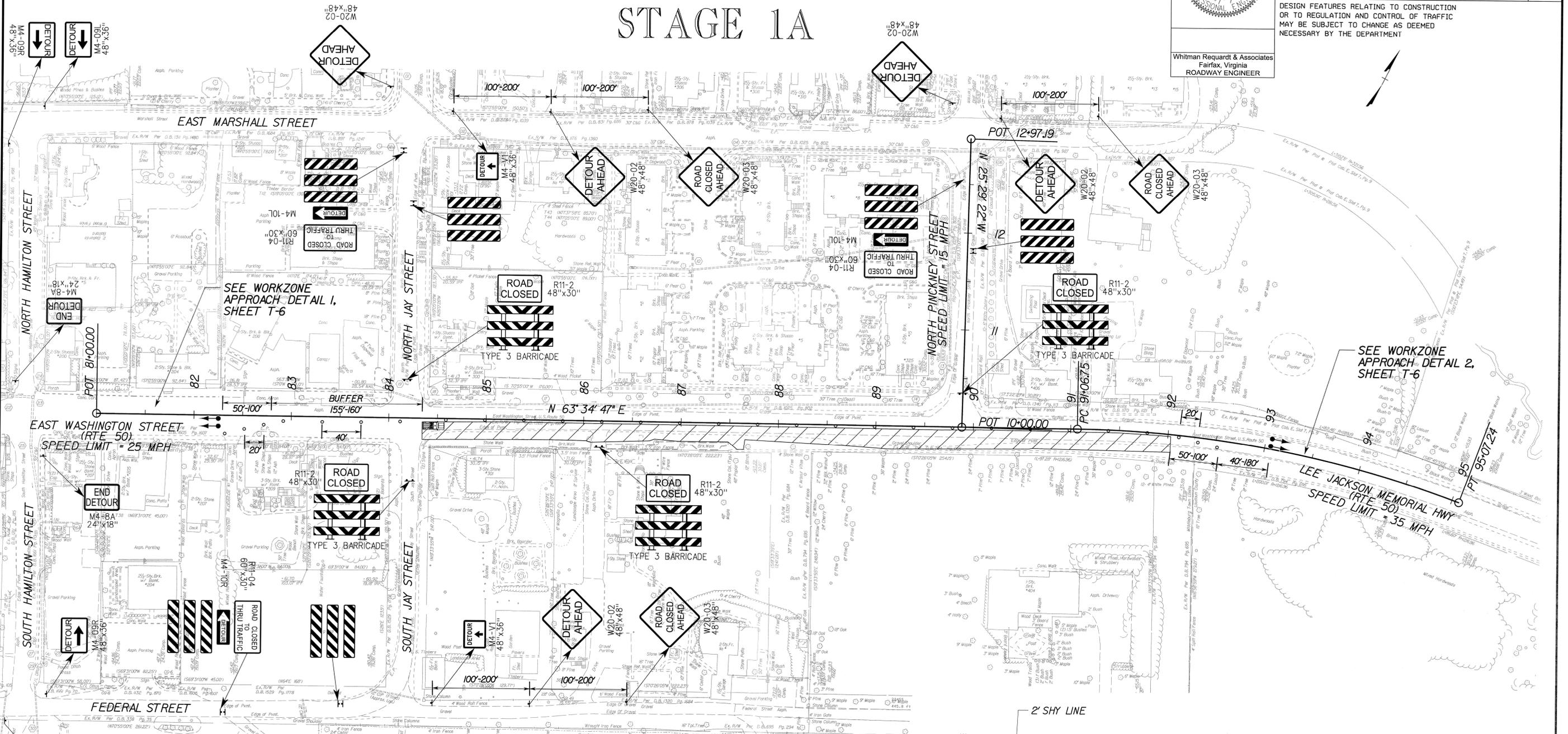
TRANSPORTATION MANAGEMENT PLAN STAGE 1A



Whitman Requardt & Associates
Fairfax, Virginia
ROADWAY ENGINEER

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	50		T-3

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

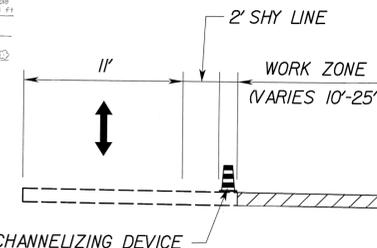


LEGEND

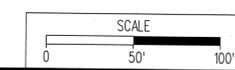
- DENOTES WORK ZONE AREA
- SHADOW VEHICLE
- GROUP 2 CHANNELIZING DEVICE
- TEMPORARY TRAFFIC CONTROL SIGNAL

NOTES:

- 1) PROJECT LIMITS SIGNING SHALL BE IN ACCORDANCE WITH TTC-53.0 OF THE WAPM.
- 2) LANE CLOSURE OPERATION ON A TWO-LANE ROADWAY USING TRAFFIC CONTROL SIGNALS SHALL BE IN ACCORDANCE WITH TTC-25.0 OF THE WAPM.
- 3) STREET CLOSURE OPERATION WITH DETOUR SHALL BE IN ACCORDANCE WITH TTC-34.0 OF THE WAPM.
- 4) CONTRACTOR SHALL MAINTAIN ACCESS TO ENTRANCES THROUGHOUT DURATION OF CONSTRUCTION.
- 4) SEE SHEET T-6 FOR ALL APPLICABLE APPROACH DETAILS.
- 5) ALL LANE CLOSURE OPERATIONS SHALL TAKE PLACE DURING NON-PEAK HOURS. AT THE END OF THE SHIFT, TRAFFIC WILL BE OPENED IN BOTH DIRECTIONS.



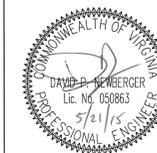
GROUP 2 CHANNELIZING DEVICE
EAST WASHINGTON ST. (RTE. 50)
SPEED LIMIT = 25/35 MPH
LANE CLOSURE OPERATION (TTC-25.0)
STA. 84+35.00 to STA. 92+03.00



PROJECT	SHEET NO.
	T-3

PROJECT MANAGER - MARTHA SEMMES (540) 687-5152
SURVEYED BY, DATE RICE ASSOCIATES (703) 968-3200
DESIGN BY - WHITMAN, REQUARDI & ASSOCIATES, LLP (703) 293-9277
SUBSURFACE UTILITY BY, DATE ACUMARK (703) 635-3074

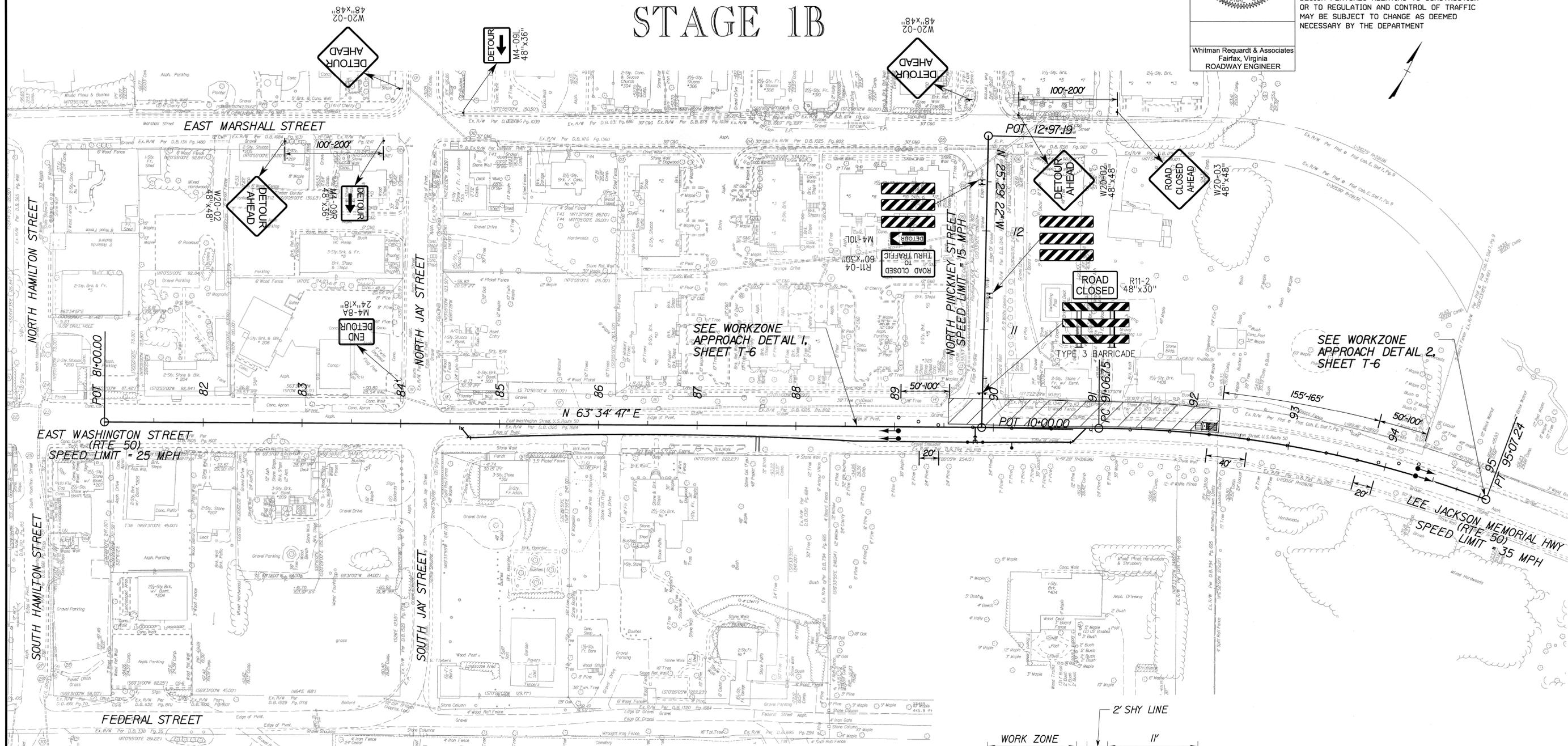
TRANSPORTATION MANAGEMENT PLAN STAGE 1B



Whitman Requard & Associates
Fairfax, Virginia
ROADWAY ENGINEER

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	50		T-4

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



SEE WORKZONE APPROACH DETAIL 1, SHEET T-6

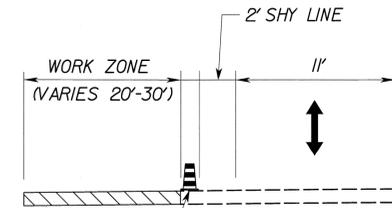
SEE WORKZONE APPROACH DETAIL 2, SHEET T-6

LEGEND

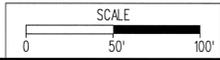
- DENOTES WORK ZONE AREA
- SHADOW VEHICLE
- GROUP 2 CHANNELIZING DEVICE
- TEMPORARY TRAFFIC CONTROL SIGNAL

NOTES:

- 1) PROJECT LIMITS SIGNING SHALL BE IN ACCORDANCE WITH TTC-53.0 OF THE WAPM.
- 2) LANE CLOSURE OPERATION ON A TWO-LANE ROADWAY USING TRAFFIC CONTROL SIGNALS SHALL BE IN ACCORDANCE WITH TTC-25.0 OF THE WAPM.
- 3) STREET CLOSURE OPERATION WITH DETOUR SHALL BE IN ACCORDANCE WITH TTC-34.0 OF THE WAPM.
- 4) CONTRACTOR SHALL MAINTAIN ACCESS TO ENTRANCES THROUGHOUT DURATION OF CONSTRUCTION.
- 4) SEE SHEET T-6 FOR ALL APPLICABLE APPROACH DETAILS.
- 5) ALL LANE CLOSURE OPERATIONS SHALL TAKE PLACE DURING NON-PEAK HOURS. AT THE END OF THE SHIFT, TRAFFIC WILL BE OPENED IN BOTH DIRECTIONS.



GROUP 2 CHANNELIZING DEVICE
EAST WASHINGTON ST. (RTE. 50)
SPEED LIMIT = 25/35 MPH
LANE CLOSURE OPERATION (TTC-25.0)
STA. 89+50.00 TO STA. 92+25.00



PROJECT	SHEET NO.
	T-4

PROJECT MANAGER - MARTHA SEMMES (540) 687-5152
SURVEYED BY DATE RICE ASSOCIATES (703) 968-3200
DESIGN BY - WHITMAN, REQUARDT & ASSOCIATES, LLP (703) 293-9717
SUBSURFACE UTILITY BY DATE ACCUMARK (703) 635-3074

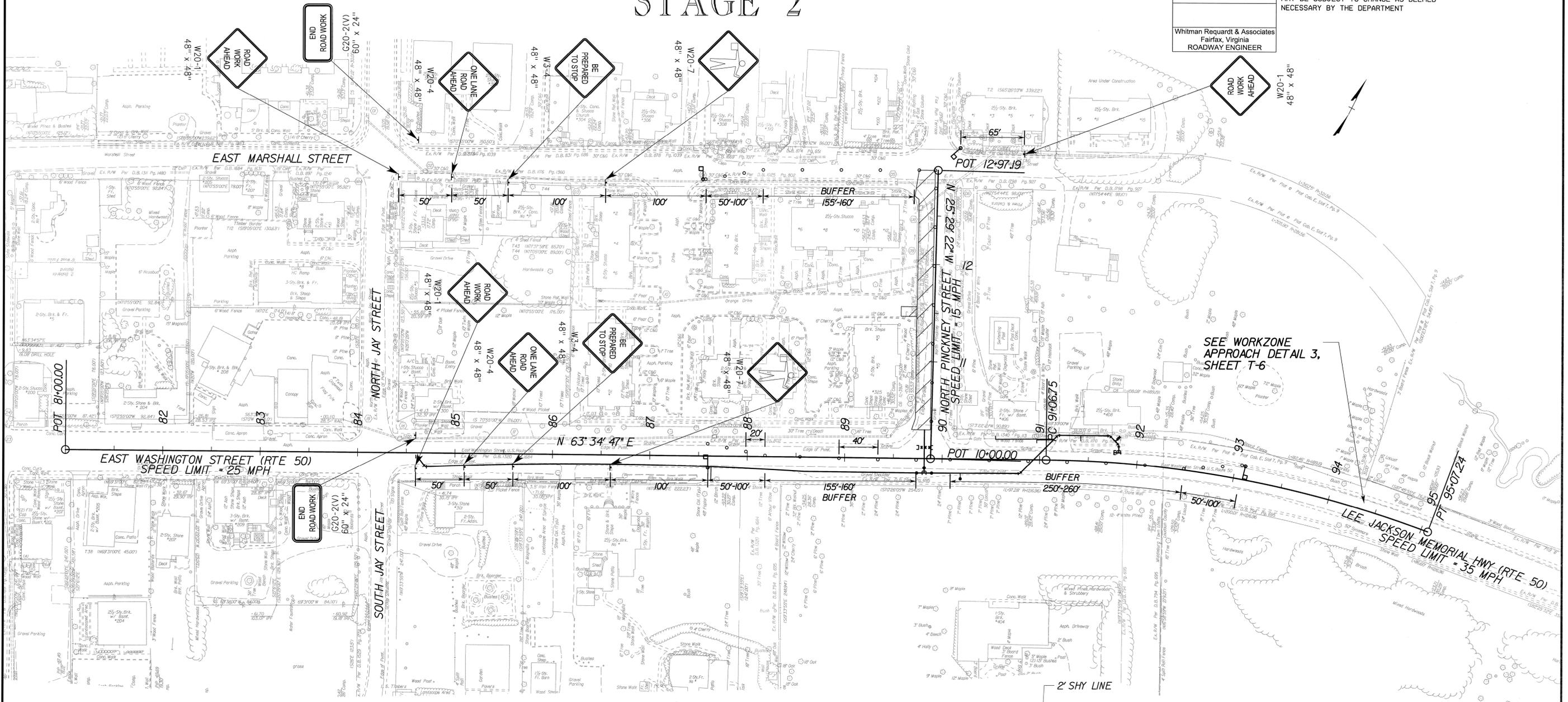
TRANSPORTATION MANAGEMENT PLAN STAGE 2



Whitman Requardt & Associates
Fairfax, Virginia
ROADWAY ENGINEER

REVISED	STATE	ROUTE	STATE	PROJECT	SHEET NO
	VA.	50			T-5

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



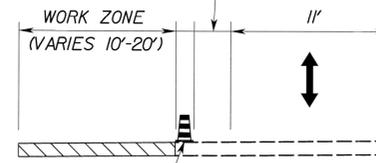
SEE WORKZONE APPROACH DETAIL 3, SHEET T-6

LEGEND

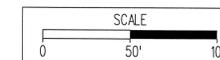
- DENOTES WORK ZONE AREA
- SHADOW VEHICLE
- GROUP 2 CHANNELIZING DEVICE
- FLAGGER STATION

NOTES:

- 1) PROJECT LIMITS SIGNING SHALL BE IN ACCORDANCE WITH TTC-53.0 OF THE WAPM.
- 2) TEMPORARY LANE CLOSURE ON A TWO LANE ROADWAY WITH A FLAGGING OPERATION SHALL BE IN ACCORDANCE WITH TTC-23.0 OF THE WAPM.
- 3) TEMPORARY LANE CLOSURE OPERATION IN AN INTERSECTION SHALL BE IN ACCORDANCE WITH TTC-28.0 OF THE WAPM.
- 4) CONTRACTOR SHALL MAINTAIN ACCESS TO ENTRANCES THROUGHOUT DURATION OF CONSTRUCTION.
- 5) SEE SHEET T-6 FOR ALL APPLICABLE APPROACH DETAILS.
- 6) ALL FLAGGING OPERATIONS SHALL TAKE PLACE DURING NON-PEAK HOURS. AT THE END OF THE SHIFT, TRAFFIC WILL BE OPENED IN BOTH DIRECTIONS.



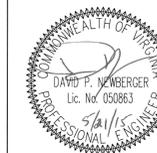
NORTH PINCKNEY STREET
SPEED LIMIT - 15 MPH
FLAGGING OPERATION (TTC-23.0)
STA.10+00.00 TO STA.12+97.19



PROJECT	SHEET NO
	T-5

PROJECT MANAGER: MARTHA SEMMES (540) 687-5152
SURVEYED BY, DATE: RICE ASSOCIATES (703) 968-3200
DESIGN BY: WHITMAN, REQUARDT & ASSOCIATES, LLP (703) 293-9772
SUBSURFACE UTILITY BY, DATE: ACCUMARK (703) 635-3074

TRANSPORTATION MANAGEMENT PLAN APPROACH DETAILS



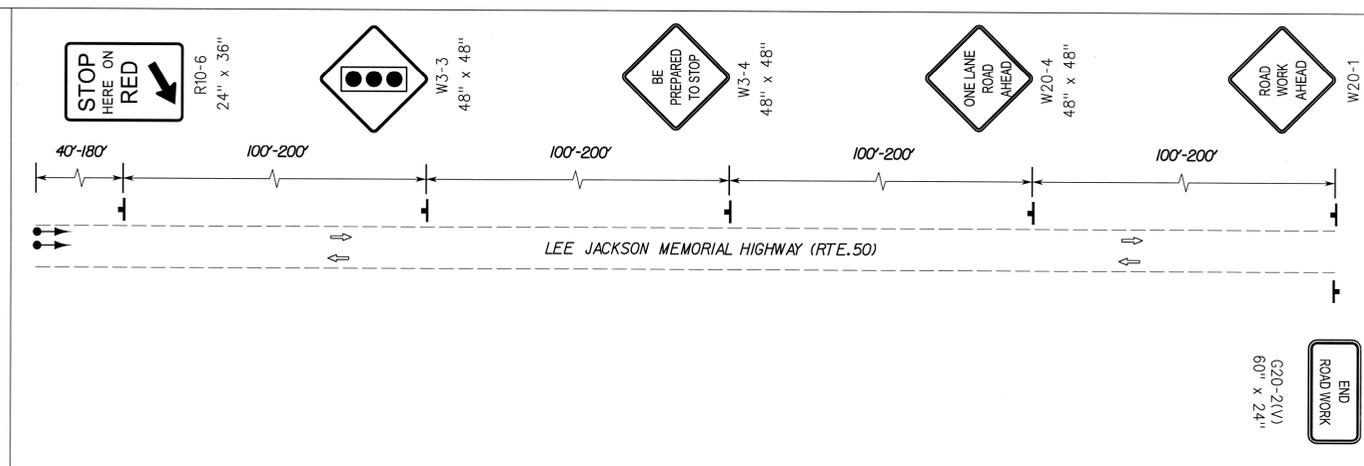
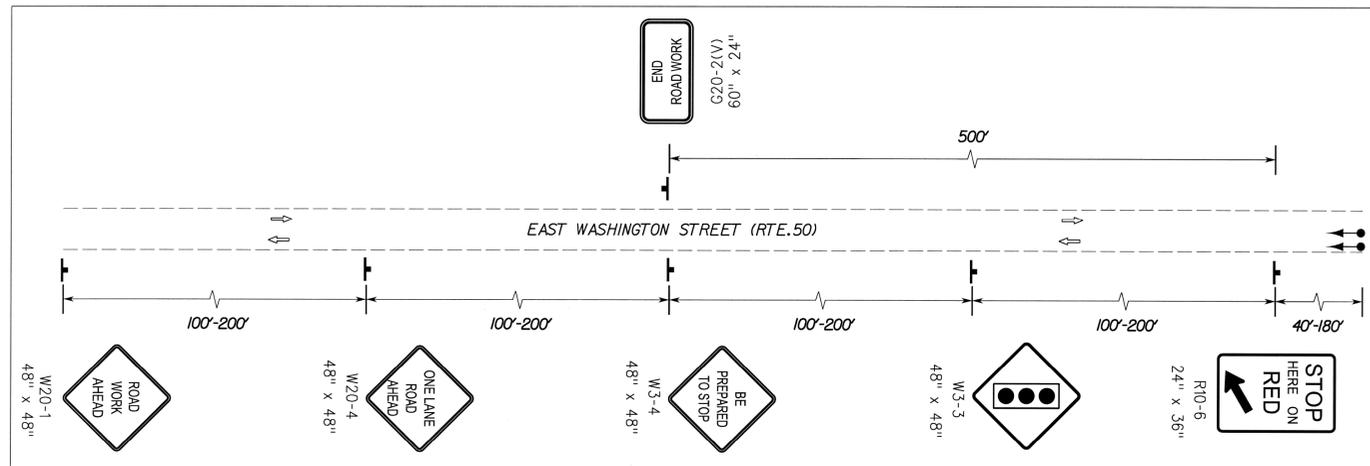
Whitman Requardt & Associates
Fairfax, Virginia
ROADWAY ENGINEER

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	50		T-6

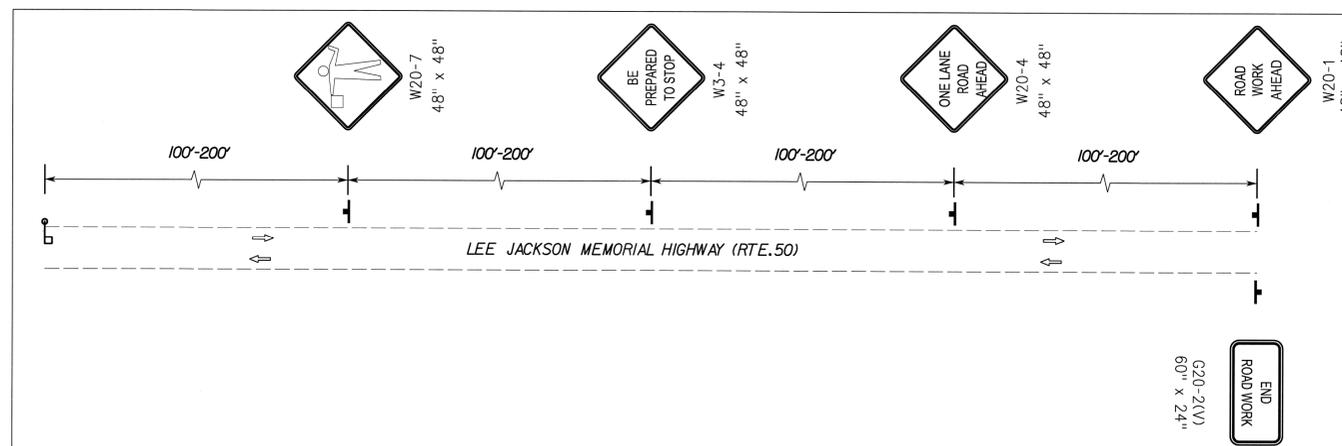
DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

APPROACH DETAIL 1
(FIGURE TTC-25.0)
N.T.S.

APPROACH DETAIL 2
(FIGURE TTC-25.0)
N.T.S.



APPROACH DETAIL 3
(FIGURE TTC-23.0)
N.T.S.



LEGEND

- TEMPORARY TRAFFIC CONTROL SIGNAL
- FLAGGER STATION

PROJECT	SHEET NO.
	T-6